PORTHCAWL WATERFRONT REGENERATION DESIGN AND ACCESS STATEMENT

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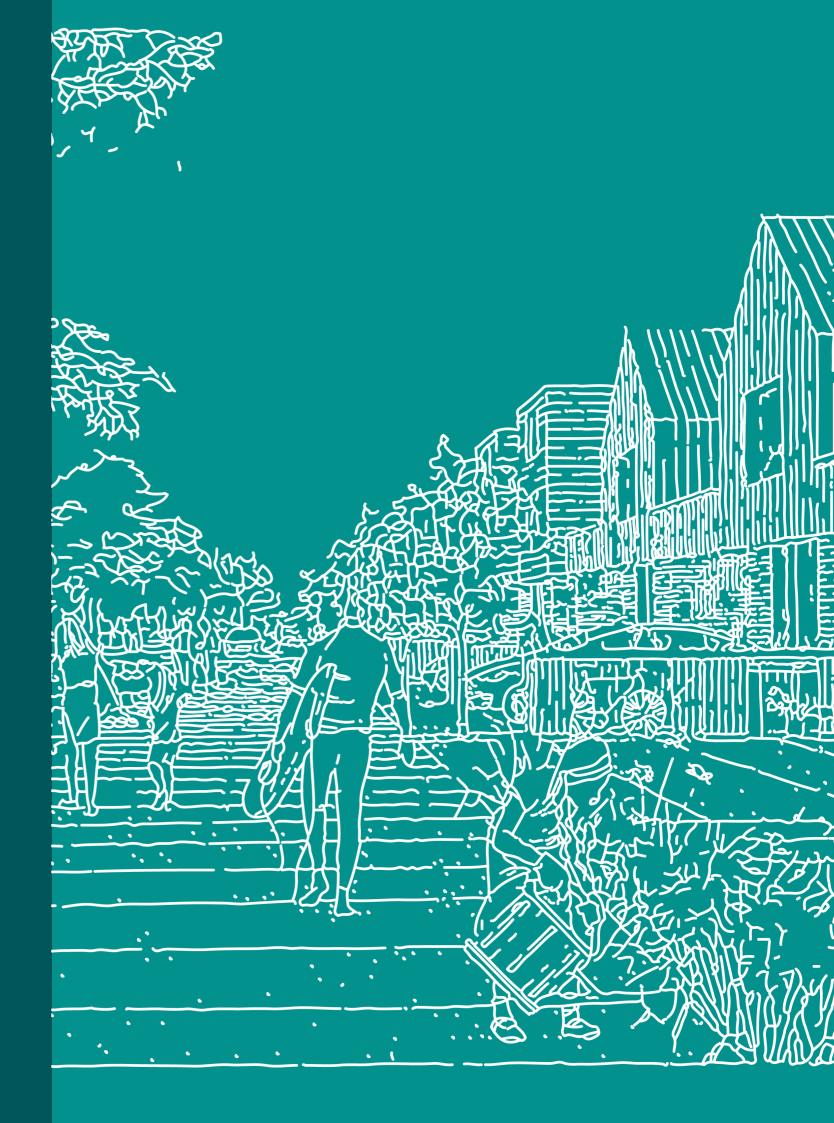
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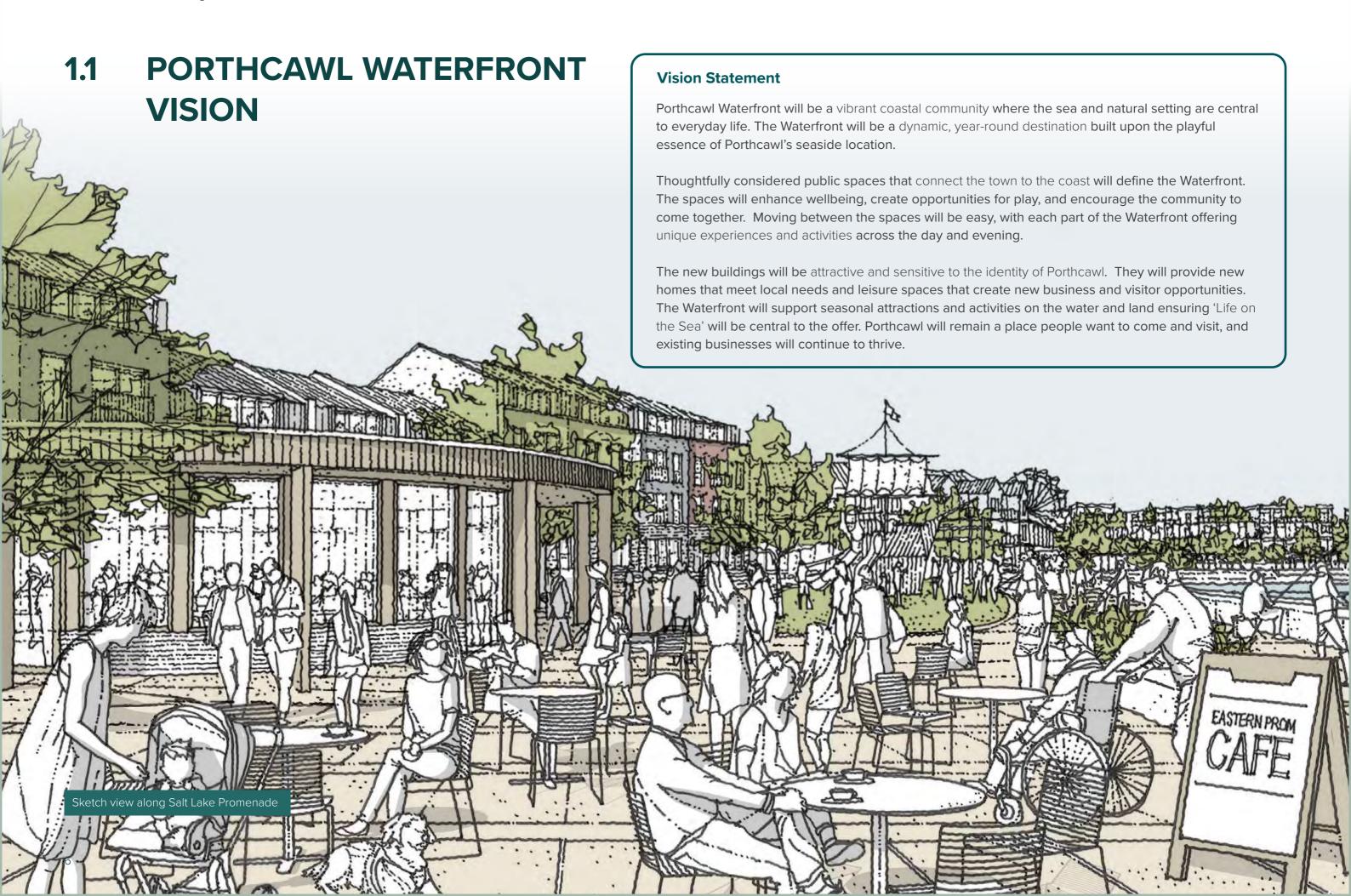
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Themes and Objectives

The vision for Porthcawl Waterfront is shaped by five core themes, which set the agenda for the masterplan and define clear objectives to guide the creation of a distinctive, high-quality place. These themes are set out adjacent.



01. Natural Setting

Enhancing Porthcawl's unique setting

- 1. Maximise opportunities for interaction with Sandy Bay Beach and the sea, providing stronger accessibility along the coastal edge for all.
- 2. Draw the natural setting into Porthcawl Waterfront, nestling the built environment into new and existing green spaces.
- 3. Improve connections between Monster Park and Griffin Park, enhancing through provision of additional amenity space and creating strong green corridors.
- 4. Improve visual connections to surrounding natural elements such as inland hills, dunes, sea and horizon, capitalising on green and blue assets.
- Introduce a variety of new and diverse spaces that enhance and complement the existing natural setting; supporting health and wellbeing.



02. People and Community

Encouraging social integration in Porthcawl

- . Support and accommodate the integration of existing and future residents and users.
- Ensure a clear hierarchy of streets and spaces that foster social interaction and a sense of ownership for future residents and the wider local community.
- 3. Create environments that encourage positive interaction and connection between locals and visitors, generating a sense of unity and belonging for everyone who uses the space.
- 4. Build upon Porthcawl's established independent businesses, creating spaces that will allow them and new complementary businesses and activities to flourish.
- 5. Generate a well integrated and varied offer for Porthcawl Waterfront that includes a combination of community uses, leisure, tourism and day to day working.



03. Connections

Improving permeability across Porthcawl

- 1. Establish new sustainable green infrastructure connections through Salt Lake, Coney Beach and Sandy Bay, designed to benefit people and the planet, making walking and cycling convenient and attractive.
- 2. Strengthen the physical connections between Porthcawl Town Centre and the sea, leveraging the coastal environment's natural beauty to enrich the town's appeal.
- 3. Establish inclusive and people-friendly connections to make Porthcawl Waterfront accessible for all, by enhancing mobility, safety and comfort for residents and visitors of all ages and abilities.
- Ensure that Porthcawl Waterfront seamlessly ties in with existing streets and spaces to provide safe physical connections between East and West Porthcawl where people take priority.
- 5. Enhance connectivity along the coastal edge with welcoming and attractive pedestrian routes that enhance the Welsh Coastal Path and support the enjoyment of the unique environment.



04. Identity

Building on Porthcawl as a place

- 1. Foster distinguishable character areas throughout the waterfront that ensure a strong sense of place and unique and vibrant urban environments that are instantly recognisable.
- 2. Reinforce and build upon the town's existing maritime identity and built heritage by respecting and responding to existing architectural features, patterns and built forms.
- 3. Capture and amplify the playful, family spirit of Coney Beach Amusement Park through new dynamic and engaging elements and playful spaces.
- 4. Deliver environments that will resonate with the local community's identity, culture, and history; creating a destination that is authentic to Porthcawl and to Wales.
- 5. Ensure a sense of 'civic-ness' by drawing on the inspiring traditional elements that are associated with great seaside town architecture.



05. Destination

Positioning Porthcawl as a seaside town

- Establish Porthcawl Waterfront as a year-round destination that can be enjoyed in all conditions and for a wide range of demographics utilising adaptable design and versatile amenities.
- 2. Foster a dynamic night-time economy that attracts both locals and visitors to spend time in Porthcawl after dark.
- 3. Transform Porthcawl into the pinnacle of an inclusive destination that is welcoming and usable to people of all ages and abilities.
- 4. Develop a series of recognisable 'niche' spaces or mini destinations; offering a range of experiences and amenities.
- 5. Build upon Porthcawl's positive links to water-sports and activities, improving the current offer and complementing with additional facilities.

1.2 PORTHCAWL IN BRIEF

Illustrative Masterplan Proposals

The following indicative layout demonstrates how up to 980 homes may be organised across the site together with a range of commercial, leisure and community facilities.

The design strategy is underpinned by a landscape-led approach that respects the coastal setting, prioritises placemaking, and creates a clear sense of identity and belonging. The development has been shaped by robust technical analysis, extensive engagement with stakeholders, and a commitment to delivering long-term value for the town and its environment. Key features of the proposals include:

- Up to 980 homes, (minimum 35% affordable with aspiration up to 50%);
- A mix of house types, sizes and arrangements to suit a variety of needs;
- Inclusion of community facilities and commercial facilities where viable;
- 2.2 ha of potential land allocated for educational use;
- Retention, expansion and enhancement of Sandy Bay Dunes;
- Creation of a new large linear park at the heart of the site via the retention, expansion and enhancement of Griffin Park, linking to a revitalised and accessible Monster Park;
- Creation of multifunctional landscape 'corridors' providing valuable recreation spaces and green outlooks for future residents;
- Extensive public open space provision incorporating equipped and informal play areas;
- Creation of a new pedestrian/cycle to realign the Wales Coast Path to follow a continuous route along the seafront;
- Utilisation of SUDS strategy fully integrated into place-making strategy; and,
- A contextually inspired and responsive approach to form and massing with particular regard to the Site's culture and history.



1.3 PROPOSALS BENEFITS

New beach
facilities including
showers, changing
rooms, storage
and toilets





Expansion,

Up to 980 new homes

300%+
increase in park
area including
enhancement of
Griffin Park

4km+ of new active travel routes connecting new and existing communities

Up to 2.2ha
area for the
creation of future
educational
provision



1.27ha of new play spaces including of equipped play areas and a new pump track



Approx
20ha of
Public Open
Space



Approx
130,000sqft of
Commercial Space
bringing new food/
drink and retail
outlets to the
waterfront

Redirecting Wales
Coastal Walk to
enable easier and
more direct route
via attractive
waterfront
promenades

New Lido and Gym/ Studio Space







2.1 PURPOSE OF DOCUMENT

This Design and Access Statement has been prepared in support of a planning application for the comprehensive regeneration of a 43.6 hectare waterfront site in Porthcawl. The proposal seeks to deliver a vibrant, sustainable, and inclusive mixed-use development, comprising of up to 980 new homes, extensive green infrastructure, enhanced public open spaces and play areas, new and improved active travel routes, and a network of commercial and mixed-use spaces, all integrated sensitively within the coastal and dune-edge context.

The purpose of this document is to outline the design rationale and approach taken in the evolution of the masterplan, demonstrating how the development responds to the unique landscape, ecological, and heritage context of the site. It explains how the proposals have been shaped by key planning and design principles, national and local policy guidance, stakeholder engagement, and the ambition to create a distinctive, resilient, and well-connected coastal community.

In particular, the statement sets out how the design provides:

- A high-quality residential environment with a mix of housing types and tenures;
- A strong and legible urban structure with active frontages and accessible public realm;
- Strategic green infrastructure that enhances biodiversity, supports surface water management, and creates valuable recreational assets;
- The protection, enhancement, and long-term stewardship of the adjacent dune system and other sensitive natural features;
- Safe and inclusive movement networks prioritising walking, cycling, and access to public transport;
- A vibrant mix of commercial, community, and leisure uses that support local economic growth and social vitality.

This document forms an integral part of the planning submission, providing clarity on how issues of design, access, and sustainability have been considered throughout the development process and how they are embedded in the proposed scheme.



2.2 STRATEGIC OVERVIEW

The regeneration of this 43.6-hectare site on the waterfront of Porthcawl represents a opportunity to transform a strategically important coastal location. Located at the interface of town and sea, the site forms a critical part of Porthcawl's wider growth and renewal strategy, delivering significant social, environmental, and economic benefits for both existing and future residents.

This mixed-use development seeks to respond positively to national and local policy ambitions, including those set out in Future Wales, Planning Policy Wales (PPW), and the Bridgend Local Development Plan. It aligns with the Welsh Government's sustainable placemaking agenda by promoting well-being, biodiversity, climate resilience, and high-quality design.

The site is allocated in Bridgend's Replacement Local Development Plan as a major, regeneration-led mixed-use site intended to deliver a comprehensive package of homes, community facilities and supporting infrastructure. The allocation envisages a significant amount of new homes, provision for future educational use, substantial public open space and a range of commercial/leisure uses to strengthen Porthcawl's tourism and local economy.

The Porthcawl Placemaking Strategy provides a comprehensive framework to guide the delivery of the Waterfront Regeneration Area, ensuring that future development reflects Porthcawl's coastal identity, heritage and sense of place. The strategy sets out a clear vision for a vibrant, mixed-use waterfront that integrates new neighbourhoods with an enhanced seafront, improved public spaces and better pedestrian and cycle connections. It identifies key development zones, including Sandy Bay, Salt Lake and Coney Beach, and establishes design principles focused on quality, sustainability and community wellbeing. Through this framework, the strategy seeks to deliver a cohesive town extension that complements the existing urban fabric, supports year-round activity, and enhances Porthcawl's role as a premier coastal destination.



2.3 FRAMEWORKS AND COMPLIANCE

The following outlines key national and local frameworks relevant to the site area. It summarises the main provisions of strategies and planning guidance, including Planning Policy Wales (ed 12) and the Well-being of Future Generations (Wales) Act, together with the pertinent policies within Bridgend's Replacement Local Development Plan (RLDP) that guide and support the regeneration of the Porthcawl waterfront.

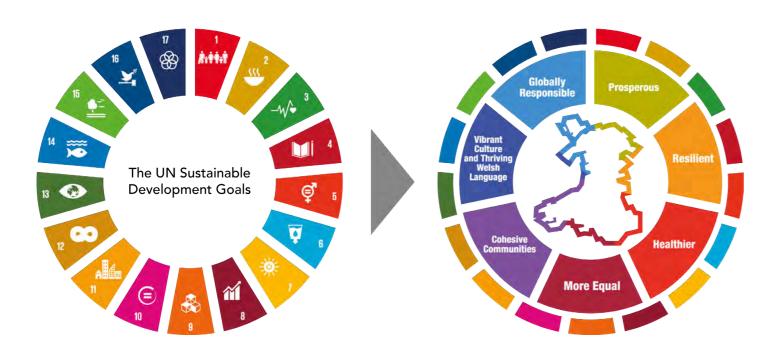
Well-being of Future Generations (Wales) Act

The United Nations' 17 Sustainable Development Goals (SDGs), established in 2015, set out an ambitious global agenda to promote prosperity, protect the planet, and improve the well-being of people worldwide.

The Well-being of Future Generations (Wales) Act, enacted in 2015, was designed to translate the United Nations Sustainable Development Goals into the Welsh context. The Act places a statutory duty on Welsh Ministers and other public bodies to set well-being objectives and take reasonable steps to achieve them, guided by the overarching principle of sustainable development.

Section 4 of the Act outlines seven well-being goals that relevant bodies are expected to work towards:

- A prosperous Wales
- A resilient Wales
- A healthier Wales
- A more equal Wales
- A Wales of cohesive communities
- A Wales of vibrant culture and thriving Welsh language
- A globally responsible Wales



United Nations Sustainable Development Goals and the Seven Well-being Goals



PLANNING POLICY WALES (PPW) EDITION 12 FEBRUARY 2024

The primary objective of Planning Policy Wales (Edition 12, 2024) is to ensure that the planning system contributes to sustainable development and enhances the social, economic, environmental, and cultural well-being of Wales. This aligns with the Planning (Wales) Act 2015, the Well-being of Future Generations (Wales) Act 2015, and other relevant legislation. PPW provides overarching land use planning policies and is supported by Technical Advice Notes (TANs), Welsh Government Circulars, policy clarification letters, and Cadw guidance.

Chapters of particular relevance to the Porthcawl Waterside Development Area include:

Chapter 2 – People and Places: Achieving Wellbeing through Placemaking

 Emphasises placemaking as a key mechanism for maximising well-being and creating sustainable, inclusive, and vibrant places.

Chapter 3 – Strategic and Spatial Choices

 Guides strategic decision-making on the location and form of development, supporting cohesive communities and resilient places.

Chapter 4 – Active and Social Places

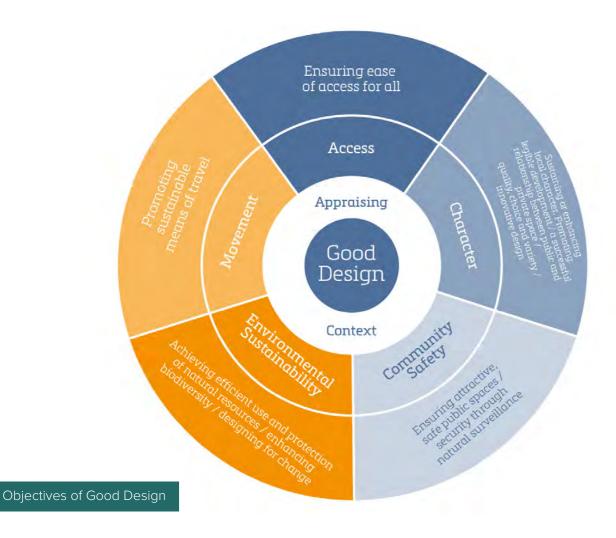
 Focuses on promoting active lifestyles, accessibility, movement within and between places, and the provision of social infrastructure that supports community life.

Chapter 5 – Productive and Enterprising Places

 Covers economic infrastructure, energy, and the efficient use of material resources, including the promotion of a circular economy.

Chapter 6 - Distinctive and Natural Places

 Highlights the importance of recognising and protecting the distinctive environmental and cultural characteristics of places, supporting both biodiversity and heritage. These chapters provide the policy context for the regeneration of the Porthcawl waterfront, guiding development to achieve sustainable, well-designed, and well-connected communities that align with national planning objectives.







FUTURE WALES: THE NATIONAL PLAN 2040

The National Development Framework (NDF) sets out a strategic vision for Wales, addressing key issues that influence the nation's prosperity and wellbeing, including the economy, housing, transport, energy, and the environment. It identifies locations for national developments, key growth areas, and the infrastructure and services required to support them. Framed around a 2040 vision, the NDF aims to deliver sustainable places across Wales by promoting placemaking, directing development to appropriate locations, optimising resource use, fostering accessible and healthy communities, protecting the environment, and supporting long-term prosperity for residents.

Relevant sections for the Porthcawl Waterside context include:

- Chapter 3 Future Wales' Outcomes
- Chapter 4 Strategic and Spatial Choices: Future Wales' Spatial Strategy
- Policy 2 Shaping Urban Growth and Regeneration: Strategic Placemaking
- Policy 6 Town Centre First
- Policy 11 National Connectivity
- Policy 12 Regional Connectivity
- Policy 19 Strategic Policies for Regional Planning
- Policy 36 South East Metro

These provisions provide a framework for guiding sustainable growth and regeneration, including the strategic development of urban areas such as Porthcawl.

ACTIVE TRAVEL (WALES) ACT 2013

The legislation places a duty on Welsh Ministers and Local Authorities to take reasonable steps to enhance provision for, and consider the needs of, walkers and cyclists. It also requires that new road schemes integrate the needs of pedestrians and cyclists at the design stage. The Welsh Government's active travel agenda seeks to encourage more people to walk, cycle, and use other active modes of transport, in order to:

- Promote the health benefits of active travel;
- Reduce greenhouse gas emissions;
- Address poverty and social disadvantage; and
- Support sustainable economic growth.

When planning and designing active travel routes, it is essential to consider:

- The creation of a coherent network that connects routes along key desire lines;
- The needs of all users, including children, young people, older people, and those with disabilities, and to allow for anticipated future use;
- Opportunities to reduce motor traffic speeds and volumes, and limit the size of vehicles along active travel routes;
- Street upgrades that improve conditions for all road users without disadvantaging pedestrians or cyclists; and
- Reallocation of road space from motor traffic to enhance pedestrian and cyclist safety and convenience.

Access to public transport hubs should be supported by high-quality, well-signed walking routes that are direct, well-lit, secure, and properly maintained. Adjustments such as relocating or introducing bus stops may be necessary to reduce walking distances and improve accessibility.

TECHNICAL ADVICE NOTES (TAN)

Technical Advice Notes (TANs) provide detailed national planning guidance on specific topics and act as supplementary documents to Planning Policy Wales. For the Porthcawl Waterside Development Area, the most relevant TANs include:

- TAN 2 Planning and Affordable Housing
- TAN 4 Retail and Commercial Development
- TAN 5 Nature Conservation and Planning
- TAN 12 Design
- TAN 13 Tourism
- TAN 14 Coastal Planning
- TAN 15 Development and Flood Risk
- TAN 16 Sport, Recreation and Open Space
- TAN 18 Transport
- TAN 23 Economic Development
- TAN 24 The Historic Environment

These TANs provide essential guidance on delivering sustainable, well-designed, and resilient development, covering housing, economic growth, environmental protection, transport, and community infrastructure, all of which are critical to the successful regeneration of the Porthcawl waterfront.

CADW BEST PRACTICE GUIDANCE

Cadw has produced best-practice guidance to complement Planning Policy Wales and TAN 24 (The Historic Environment). This guidance is informed by Cadw's Conservation Principles, which set out the organisation's approach to the protection and management of the historic environment.

For the Porthcawl Waterside Development Area, the following Cadw documents are particularly relevant, given that projects identified in the Bridgend Railway Station Area Options Appraisal (December 2021) include listed buildings that partly fall within the conservation area boundary:

- Conservation Principles: for the sustainable management of the historic environment in Wales (March 2011)
- Managing Change to Listed Buildings in Wales (May 2017)
- Heritage Impact Assessment in Wales (May 2017)
- Managing Conservation Areas in Wales (May 2017)
- Managing Historic Assets in Wales (May 2017)

HISTORIC ENVIRONMENT (WALES) ACT 2016

The historic environment in Wales is protected and sustainably managed through legislation that provides enhanced safeguards for listed buildings, establishes historic environment records for local authority areas, and creates an Advisory Body for the Welsh Historic Environment.

A portion of the Porthcawl Waterside Development Area falls within the Porthcawl Conservation Area, making this legislation particularly relevant. The Planning (Listed Buildings and Conservation Areas) Act 1990, as amended by the Historic Environment (Wales) Act 2016, provides the statutory framework for

- The designation of areas of special architectural or historic interest as conservation areas;
- The periodic formulation and publication of proposals to preserve and enhance conservation areas:
- Ensuring that planning decisions give special attention to the desirability of preserving or enhancing the character or appearance of a conservation area;

- Requiring conservation area consent for the demolition of unlisted buildings within a conservation area; and
- Allowing for urgent works to buildings within a conservation area.

Within the historic marina area of Porthcawl, there are several listed buildings and structures, including Jennings Buildings, the Custom House, Outer Basin Walls, the Breakwater, and the Lighthouse. Additionally, Newton Primary School is also listed, reinforcing the need for careful consideration of heritage assets in the planning and design of redevelopment proposals.



Placemaking Wales Charter

The Placemaking Wales Charter sets out a framework for delivering high-quality, sustainable, and people-focused places across Wales. It builds on the principles of Planning Policy Wales and the Wellbeing of Future Generations (Wales) Act, promoting an integrated approach to planning, design, and development that considers social, economic, environmental, and cultural well-being.

The Charter encourages all public and private sector stakeholders to:

- Place people at the centre of decision-making, ensuring communities are actively engaged in shaping their local environment;
- Deliver development that is safe, inclusive, accessible, and resilient, supporting health and well-being;
- Protect and enhance the distinctive character and heritage of places, integrating high-quality design with local identity;
- Promote sustainable economic growth and create opportunities for prosperity while reducing environmental impact; and
- Ensure that new development contributes positively to the public realm, infrastructure, and connectivity, creating cohesive and vibrant communities.

For the Porthcawl Waterside Development Area, the Charter provides a guiding framework for regeneration, emphasising placemaking principles that support well-connected, attractive, and sustainable waterfront communities while respecting the area's historic and cultural assets.

Rooted in the principles of the Placemaking Wales Charter, the proposals set out within the Design and Access Statement (DAS) place a strong emphasis on creating high-quality public spaces, promoting active travel, and enhancing environmental resilience. The vision seeks to ensure that Porthcawl continues to be a welcoming, vibrant, and sustainable place for residents, businesses, and visitors alike.

The Placemaking Wales Charter identifies the potential to deliver high-quality development and public spaces through three key components; Activity, Physical Form, and Meaning. Together these form the foundation for successful placemaking. These components are expressed through six interrelated aspects of development: Movement, Mix of Uses, Location, Public Realm, People and Community, and Identity.



BCBC is signatory to Placemaking Wales Charter



BRIDGEND COUNTY BOROUGH COUNCIL'S CORPORATE PLAN FOR 2023-28

Published in April 2023 and titled 'Delivering Together', Bridgend County Borough Council's Corporate Plan sets out the authority's main priorities for the next five years. The plan outlines seven well-being objectives and demonstrates how these align with the goals of the Well-being of Future Generations (Wales) Act 2015.

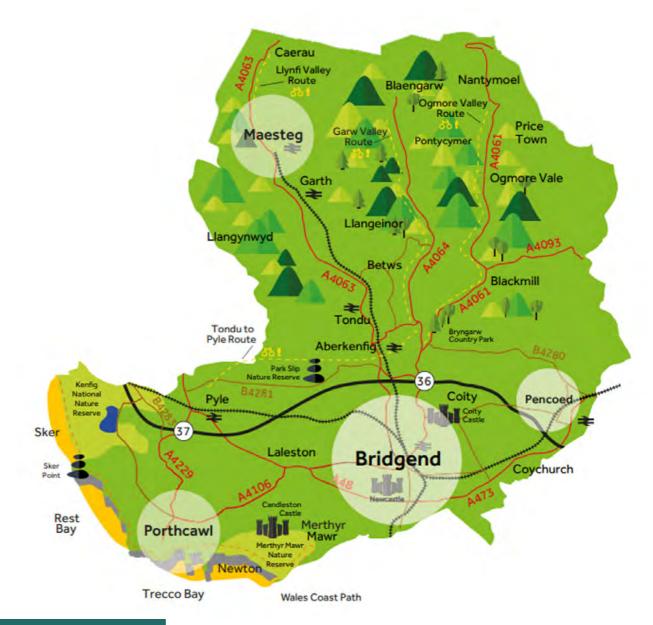
Several of these objectives are of particular relevance to the Porthcawl Waterside Regeneration, including:

- Fair work, skilled, high-quality jobs, and thriving towns;
- Responding to the climate and nature emergency;
- Making people feel valued, heard, and part of their community; and,
- Supporting people to be healthy and happy.

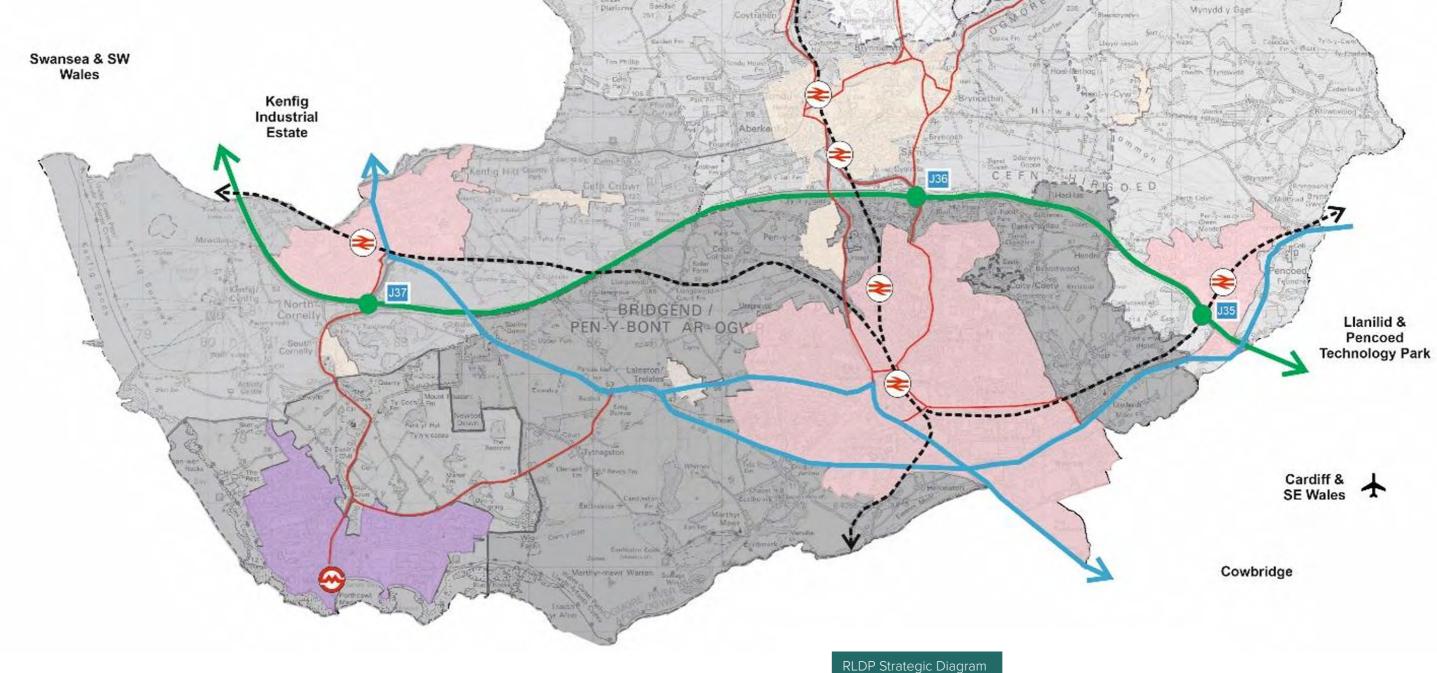
Under these overarching themes, the Corporate Plan sets out specific actions to achieve these aims, such as:

- Enhancing and revitalising town centres to make them safer, more attractive, and vibrant;
- Attracting investment and supporting both new and existing local businesses;
- Moving towards a net zero carbon future and improving the energy efficiency of council buildings and services;
- Protecting landscapes and open spaces, including increased tree planting and green infrastructure;
- Strengthening flood defences and introducing schemes to reduce flood risk to homes and businesses:
- Improving engagement with local communities, including young people, ensuring their views inform decision-making;
- Expanding and improving active travel routes and facilities to encourage walking and cycling;
- Providing accessible and engaging leisure and cultural opportunities;
- Enhancing children's play spaces and recreational opportunities; and
- Increasing the supply of affordable housing across the county borough.

Collectively, these objectives provide a strong policy foundation for the regeneration of Porthcawl's waterfront, supporting sustainable growth, improved well-being, and the creation of a resilient, connected coastal community.



Bridgend Assets Diagram



REPLACEMENT BRIDGEND LOCAL DEVELOPMENT PLAN (2018-2033)

The Replacement Bridgend Local Development Plan 2018-2033 (RLDP) was adopted by Bridgend County Borough Council and became operative on 13 March 2024.

The Replacement Plan refines the original four strategic objectives to better reflect identified local challenges, align with updated national policy, and achieve a balanced approach to sustainability. These overarching aims are supported by 35 specific objectives, reinforcing the Council's commitment to:

- High-quality design and sustainable placemaking;
- Sustainable transport and accessibility;
- Active travel; and
- Energy efficiency in the design and construction of new buildings.

While the structure and detailed wording of policies within the RLDP differ from those in the adopted 2013 LDP, the proposed changes do not require any fundamental revisions to the work undertaken to date for the Porthcawl Waterside Development Area. The Placemaking Strategy, Open Spaces Concept Design, and Infrastructure Plan have all been developed with full consideration of the emerging RLDP framework and its policy direction.



Porthcawl Placemaking Strategy

The Porthcawl Placemaking Strategy, published by Bridgend County Borough Council in 2022, provides a comprehensive framework to guide the regeneration of Porthcawl in the short, medium, and long term, identifying key areas for investment, renewal, and development. The document was prepared by architects Austin Smith Lord, who were commissioned by the Council in 2022 to undertake an in-depth analysis of the town and prepare a strategy to inform the sustainable future of Porthcawl.

The purpose of the Strategy is to establish a clear vision for the town's evolution, highlighting opportunities to enhance connectivity, open space, land use, scale, and sustainability, while ensuring that all future development aligns with the wider ambitions of the Bridgend Replacement Local Development Plan (2018–2033) and the principles of Planning Policy Wales and the Well-being of Future Generations (Wales) Act.

The study identifies six principal areas of future development, each comprising sub-areas that represent development opportunities in their own right. For each area, the Strategy sets out key criteria and recommendations to guide high-level design, infrastructure, and placemaking objectives, without prescribing specific interventions. This provides flexibility for subsequent stages of design development and planning applications while maintaining consistency with the Council's vision for the town.

The Strategy is structured around six core placemaking principles, Movement, Mix of Uses, Public Realm, People and Community, Identity, and Location, which together promote a connected, inclusive, and vibrant coastal settlement. The document also benchmarks its proposals against national policy frameworks, including the 20-Minute Neighbourhood principles and the goals of the Future Generations (Wales) Act, ensuring alignment with Wales' broader sustainability agenda.

The Placemaking Strategy serves as an essential foundation for this outline planning application. It provides a strategic framework upon which further research, analysis, and design development have been built, including the Open Spaces Concept Design and Infrastructure Plan. Collectively, these documents ensure that the emerging masterplan for the Porthcawl Waterside Development Area remains grounded in adopted local policy, reflects the town's unique character, and delivers a resilient, well-connected, and sustainable coastal community.

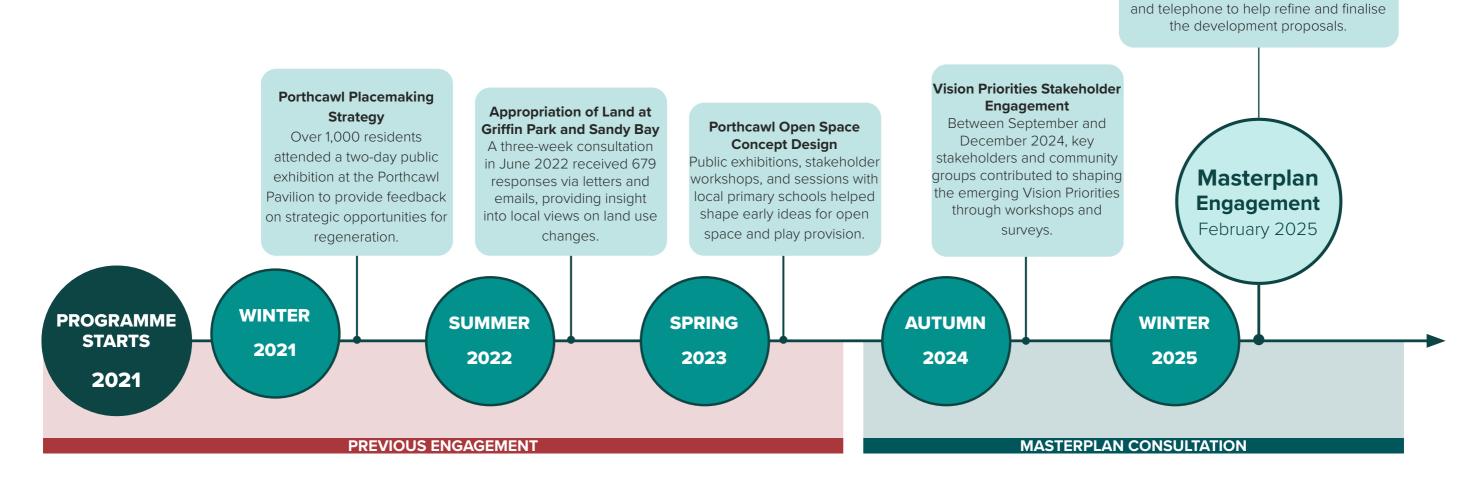




2.4 STAKEHOLDER ENGAGEMENT

A comprehensive programme of community engagement has been undertaken to ensure that redevelopment proposals reflect the aspirations and priorities of local residents and stakeholders.

Since 2021, five key consultation exercises have been carried out and are summarised below:



Masterplan Engagement

A non-statutory 30-day public consultation presented the initial masterplan to the wider

community through a public exhibition and

dedicated website. Feedback was collected

through email, forms (online and hard copy),

Engagement Summmary Timeline Diagram

These engagement activities have been instrumental in shaping the evolving masterplan, ensuring it reflects community values, supports local needs, and delivers a shared vision for the future of Porthcawl's waterfront.

The key feedback points from the Masterplan Engagement were:

- Open Space and public realm to include spaces for sport and creative, bohemian activities.
- Concerns about housing numbers, distribution, and ensuring homes are sustainable, inclusive, and support a vibrant year-round community.
- Addressing coastal challenges through climate risk assessment, resilient drainage and flood infrastructure, and strategic planting to mitigate weather impacts.
- Transport and connectivity for all users with the need for phased parking solutions, support for active travel and shared mobility.
- Tourism and economic development calling for motorhome facilities, a hotel, uses that support water sports.
- Accessible community infrastructure and local services that will support existing communities.
- Recreational and leisure areas providing more outdoor children's spaces including free activities, small promenade features, and events like skating.
- More sympathetic and traditional scale, massing and architecture for the proposals which clearer connections to the local context.
- Increased leisure offer for the area that supports the enjoyment of existing natural assets and compliments the town centre.

While there have been a wide range of opinions collated there are many shared views and areas of in-principle support. The proposal put forward as part of this outline application has been shaped directly in response the feedback compiled, reinforcing the ideas and concepts that were most well received, while significantly reworking others.









Photos from engagement events

2.5 DESIGN EVOLUTION

The evolution of the illustrative masterplan has been a collaborative and iterative process, shaped by technical analysis, stakeholder engagement, and community feedback. Building upon the foundations established by Austin Smith Lord's Placemaking Strategy, the design team undertook a structured process of review, refinement, and response to evolving priorities. Beginning with a comprehensive Baseline Review and Visioning exercise, the design has continuously adapted to reflect both the site's opportunities and the aspirations of Bridgend County Borough Council and the Welsh Government. Through successive stages of optioneering, testing, and consultation, the masterplan has matured into a robust framework, one that balances ambition with deliverability, integrates sustainability and character, and responds meaningfully to local identity and place.

The following pages outline this process in greater detail, illustrating the key stages in the masterplan's development through a timeline of milestones and design iterations. They highlight how each phase of review, technical input, and community engagement informed a gradual but significant evolution in approach; from early concept ideas to a refined framework that reflects a shared vision for Porthcawl's future.

Baseline Review

The Baseline Review and Benchmarking Report marked the starting point for Bridgend County Borough Council and Welsh Government's appointed design team, providing a vital understanding of previous work and helping to identify the site's opportunities and constraints.

Austin Smith Lord Placemaking Strategy

Architects Austin Smith Lord who were appointed in 2022 undertake an analysis of the area and prepare a placemaking strategy for Porthcawl.

The Design Team Commence with Masterplan Process

The design team initiate their design process with a Baseline Review and Benchmarking Report.

Vision and Objectives Document

A series of Vision and Objectives were developed through the analysis and review of existing works, which provided a foundation for them. Using the Benchmarking Report, a refreshed vision for Porthcawl was then created.

2022 Austin Smith Lord Placemaking Strategy July 2024 The Urbanists begin Regeneration Strategy Work August 2024 Vision and Objectives



Illustrative Masterplan
January 2025

Optioneering Process

During the optioneering process, three masterplan variations were developed, each exploring different levels of deviation from the placemaking Strategy. Elements from each were combined to form a preferred option, which, together with a landscape strategy, helped shape the early stages of the masterplan.

Illustrative Masterplan

The Illustrative Masterplan evolved from the preferred option and has since undergone amendments as a working file. It now forms the Framework Masterplan referenced throughout.



Public Engagement Event February 2025

Public Engagement

The Illustrative
Masterplan and supporting
Parameter Plans were used to
communicate the proposals and
gather feedback from the local
community and stakeholders.
This feedback was then used
to inform revisions, which
were incorporated into
the Framework
Masterplan.

Design Code

Having established a clear framework, the Design Code actively guides the site's physical development, ensuring all outcomes meet the high-quality standards and objectives.

October 2024
Options and Landscape
Strategy produced

End of 2024/January 2025 Illustrative Masterplan produced February 2025

Public Engagement

Present and Future
Design Code and starting
Planning Process

Placemaking Strategy Plan

Throughout the study, six principal areas of future development are identified which contain sub-areas that represent development opportunities in their own right. The document outlines key criteria for each area which recommends but does not detail specific works.

The main strategies cover elements of Movement, Mix of Uses, Public Realm, People and Community, Identity, and Location over four key themes, being;

- 1. Connectivity
- 2. Open Space
- 3. Mix of Uses, Distribution and Scale
- 4. Sustainability

Some of the key strategies propose:

- Improved car parking facilities at Hillsboro;
- A high quality facility for arrival by bus, linked to cycle hire and community facilities;
- Enhanced active travel routes and prioritising the pedestrian and cyclist throughout the whole development;
- A new access road to prevent vehicular access to the new Coney Beach and Sandy Bay development areas from Mackworth Road, Sandy Lane and Rhych Avenue;
- Creating high quality, flexible public realm along Salt Lake and Eastern Promenade;
- Enhanced Griffin Park and created a green link through the former Monster Park to the Relic Dunes;
- Create a variety of green spaces to accommodate a range of activities;
- Approximately 1,100 new homes of mixed tenure and typology;
- 30% of new homes to be affordable;
- Potential for 4000m² of commercial and community uses within a mixed use setting;
- Potential for new hotel;
- 1950m² new foodstore;
- A 4 Bay Bus Interchange and Cycle Hub;

- Promote principles of sustainable design and maximise the viable use of green energy technology;
- Design for biodiversity and sustainable drainage; and,
- Seek to push beyond simple compliance with legislation and take opportunities to quicken the pace of positive change towards addressing climate change through net zero carbon and net zero energy design and development initiatives.



Placemaking Strategy produced by Austin Smith Lord (2022)

Consultation Masterplan (Jan 2025)

The Masterplan development went through several iterations and an optioneering process as a result of further technical scrutiny and an evolving brief.

The key differences between the early Illustrative Masterplan and the Placemaking Strategy were as follows:

- Relocation of the Access Road: The route connecting Sandy Bay through Coney Beach and Monster Park was shifted to the southern edge. This adjustment was primarily made to create a more continuous and linear stretch of open space extending from Griffin Park.
- Repositioning of the Development Line: The
 development line along the Sandy Bay dune front
 was moved further north to accommodate a large
 attenuation basin, responding to natural dune
 movements, and providing an expanded public
 area. This space will include multiple pedestrian
 connections as an extension of the proposed
 boardwalk.
- Creation of a Green Edge: A green corridor has been introduced along the Sandy Bay development, incorporating north—south green routes to break up the built form and establishing a natural buffer between the new development, nearby residential areas, and Trecco Bay.
- Improved Visual Integration of Aldi: A new building block proposed around the Aldi store to screen its rear elevation from key viewpoints along the promenade, boardwalk, and beach.
- Retention and Enhancement of Griffin Park: The
 revised access road alignment along the southern
 edge allows for the maximum possible retention and
 enhancement of the existing Griffin Park.



Current Masterplan Proposals

Following the consultation, the January and February masterplan proposals were further developed in direct response to the feedback and comments received during the engagement process and from further technical scrutiny.

The key areas of change and further development included:

- Enhanced Leisure Offer: The masterplan now includes a broader range of leisure activities, such as an open-air pool, relocated pump track/wheel park, varied play areas, and a funfair, reflecting the historic character of the former Coney Beach amusement park. These facilities are distributed throughout the site to encourage activity across the whole area.
- Reduced Building Heights: Building heights have been lowered across several key areas, particularly within the Salt Lake blocks and the Sandy Bay apartments at the southern edge.
- Adjusted Housing Density: The reduction in building heights has naturally resulted in lower densities and a decrease in the total number of homes, addressing one of the community's most significant concerns.
- Improved Access Road Frontages: The design of frontages along the new Sandy Bay access road has been enhanced to provide greater passive surveillance and a higher-quality public realm. Parking areas have been repositioned behind buildings to ensure this key active travel route is not dominated by vehicles.
- Enhanced Beach front Facilities: New shower and changing facilities are proposed near the dunes, alongside flexible units for potential food and beverage operators, to improve the beach front experience and support visitor amenities.
- Varied Building Heights at Salt Lake: Additional variation in the height and massing of buildings within the Salt Lake area helps to break up the built form and reduce the sense of high density at street level.



Design and Access Statement

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3.1 URBAN FABRIC AND ARCHITECTURAL CHARACTER

Existing Built Character

The existing built character of Porthcawl has evolved organically over time, creating a distinctive sense of place. The headings below capture some of the key architectural and streetscape characteristics of the town.

Building Heights

Buildings are typically 2 and 3 storeys throughout the town centre and increase to 4,5,6 and 7 storeys towards the seafront.

Streetscape and Building Character

Streets are typically arranged perpendicular to the sea giving more homes views of the water beyond. There is a strong vertical rhythm to dwellings, articulated by projected bays and gables. Decorative ironwork balconies and covered porches give the buildings depth and an individual character. Many dwellings have a small front garden facing the street, creating a defensible threshold between the public and private domain. Trees are occasionally present along the streets, providing passive shading for dwellings and the public realm.

Public buildings, particularly around harbourside are simple pitched roof forms with a regular rhythm of punched windows.

Materiality and Colour

Buildings within the town are adorned with a variety of materials, including stone, brick, slate, timber and render. Within this material range, the application and colour vary, creating rich, textured and often playful façades. Colours are muted and tasteful and there is often consideration for how they work together along the entire street elevation.



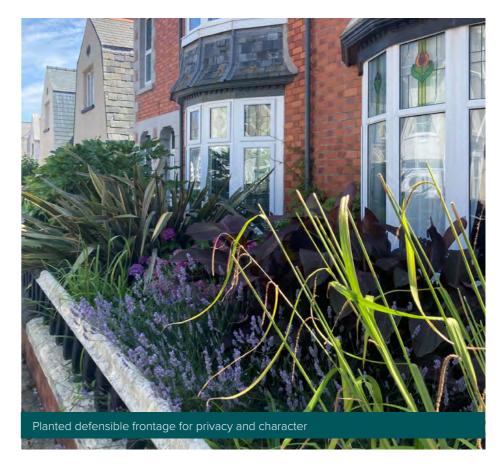




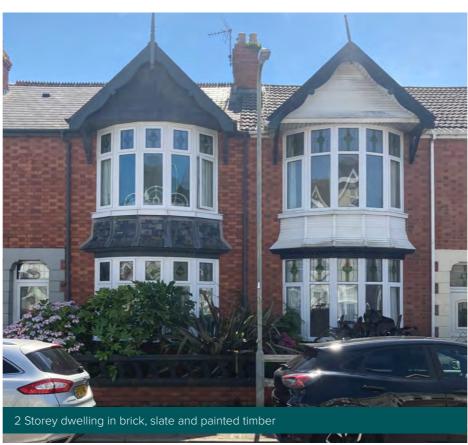














Town Centre High Street

Built Form

The built form of Porthcawl High Street consists mainly of terraced shopfronts with upper floors, typically used for storage, offices, or residential flats. Buildings are closely spaced, creating a continuous street frontage that defines the high street's character and supports a strong pedestrian environment.

Block Typology

The block typology of Porthcawl High Street follows a traditional perimeter block layout, with narrow frontages that are typically long and shallow, with active ground-floor uses (mainly retail) and residential or ancillary spaces above. Rear access is often available via service lanes or minor roads, allowing for deliveries and parking without disrupting the pedestrian-focused frontage.

Scale

The scale of Porthcawl High Street is predominantly low-rise, with most buildings ranging from two to three storeys. This modest height contributes to a human-scale environment, reinforcing the street's walkability and approachable character.

Porthcawl High Street uses fixed metal bollards to define the pedestrian zone and restrict vehicle access. These are typically spaced at regular intervals along the street's entrance and edges. These are often complemented by raised kerbs or tactile paving providing a clear physical and visual boundary



The public realm of Porthcawl High Street is designed to be pedestrian-friendly and welcoming. It features numerous wooden and metal benches, planters with seasonal flowers, and litter bins placed at regular intervals. These elements are arranged to encourage dwell time and social interaction while maintaining clear walking routes. The planters soften the streetscape, and the abundance of seating offers rest points for all ages. Overall, the design prioritises accessibility, comfort, and visual appeal within the pedestrian zone.





The roofscape of Porthcawl town centre's High Street is varied, reflecting the town's historical character and mixed-use architecture. Most buildings feature traditional pitched or gable roofs, often with slate or tiled finishes, typical of coastal towns in South Wales. Rooflines vary in height and orientation, creating a visually interesting skyline. Some properties include dormer windows or chimneys, adding further detail and a sense of rhythm to the roofscape. This variation contributes to the High Street's charm and complements the mix of shopfronts and façades below.

The High Street features a mix of materials, including stone, brick, and render, which create visual interest and character. Architectural details such as bay windows and decorative window features further enhance the variety, contributing to the overall streetscape.



New Road / Mackworth Road

Built Form

The built form along New Road and Mackworth Road is predominantly residential, with a mix of terraced, semidetached, and detached houses. On New Road, homes are generally terraced or semi-detached, set close to the street with minimal or no front gardens. In contrast, Mackworth Road features a slightly looser urban grain, with houses typically having short front gardens or larger set-backs that include driveways, giving the street a more open feel.

Block Typology

The block typology of New Road and Mackworth Road consists of linear residential blocks with long, narrow plots. Buildings are fronting directly onto the street or set back behind short gardens or driveways. Rear gardens extend behind each plot, often backing onto other properties or service lanes.

Scale

The scale along New Road and Mackworth Road is suburban, with most homes being two storeys in height. The presence of front gardens, driveways, and varied building set-backs along Mackworth Road adds a sense of openness, while the closer building lines on New Road contribute to a more enclosed feel.

The houses along New Road are positioned directly onto the street, with some featuring small front garden areas. In contrast, properties along Mackworth Road typically have short front gardens or larger garden spaces that often include driveways, providing a more open and varied frontage



BOUNDARIES

The public realm is defined by narrow pavements, with the roadway occupying the majority of the street space. Traffic tends to dominate, though traffic calming features such as speed bumps and raised, illuminated crossing points have been introduced to improve safety and slow vehicle movement.





The roofscape in the area displays a mix of architectural styles, ranging from gable-fronted roofs positioned above double-height bay windows to more traditional pitched roofs typical of terraced housing. These roof forms contribute to the area's distinctive and layered architectural identity.

The materiality of the area is diverse, featuring painted render in a range of colours and tones, alongside stonework and occasional brick façades. This combination introduces a variety of textures and finishes, adding visual interest and character to the streetscape.



Victoria Avenue / Mary Street

Built Form

The built form of Victoria Avenue and Mary Street is predominantly terraced housing, with some semi-detached and detached properties. Buildings are set back slightly from the street behind short front gardens, creating a consistent building line and a clear rhythm along the street. The overall form supports a medium-density, residential streetscape with a strong sense of enclosure and continuity.

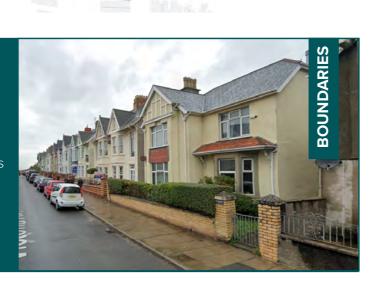
Block Typology

The block typology follows a traditional perimeter block layout, with long, narrow plots arranged in continuous terraces. Buildings front directly onto the street with small gardens separating them from the pavement, while the rear of the blocks typically features private back gardens and rear access lanes. This typology supports a clear public-private separation and creates a coherent residential grain that defines the character of the area.

Scale

The scale is modest and residential, with buildings generally rising two storeys. The consistent height across terraces creates a uniform roofline and a human-scale environment.

The boundary treatments along Victoria Avenue and Mary Street typically consist of short front gardens, enclosed by low brick or stone walls often topped with decorative metal railings. In some cases, these are complemented by hedges or planted borders, introducing soft landscaping that adds greenery and a more domestic feel to the streetscape. These treatments create a clear but welcoming separation between the public footpath and private property, contributing to the area's residential character.



Victoria Street and Mary Street feature narrow pavements, which restrict pedestrian space and contribute to a more enclosed feel. There are some areas of street greening, such as planters and small trees, adding softness to the streetscape. On-street parking is common, creating a more vehicle-dominated environment which creates limited space in some areas.





Most roofscapes in this area are defined by gable-fronted designs above double-height bay windows, creating a strong vertical rhythm along the street. Variations in roof detailing introduce visual diversity, helping to differentiate buildings and add character to the streetscape. On streets that run perpendicular to the sea, the bay windows offer framed views toward the seafront, enhancing the coastal setting and connecting the architecture to its surroundings.

The materiality along Victoria Avenue and Mary Street in Porthcawl is varied, featuring a mix of brick, render, and stone, with occasional timber detailing around windows. This combination of materials adds to the visual interest of the streetscape, highlighting the individuality of buildings while maintaining a cohesive overall character. The variation in finishes, such as cornices, window surrounds, and decorative timberwork, enhances the historic charm of the area and reflects its incremental development over time.



Esplanade

Built Form

The built form along the Esplanade is diverse, consisting of a hotel, semi-detached housing, the Grand Pavilion and multiple commercial / retail / food and beverage facilities. Ground floors are typically active and have a positive relationship with the public realm. Small food and beverage pavilions animate the promenade.







The hotel and Grand Pavilion occupy the full width of their urban blocks with terrace housing and apartments to the rear screening service and delivery zones. Residential blocks consist of dwellings facing the sea and animated with covered sheltered balconies and bay windows. Commercial units sit beneath apartments providing active frontages.



ROOFSCAPE

There are multiple roof types along the esplanade, including a mansard to the hotel, gables facing the street for semi-detached housing, a prominent dome to the Grand Pavilion and staggered flat roofs for apartment buildings. Pitched roofs are often animated with dormer windows facing the sea, maximising visual connections with the water from habitable spaces within.



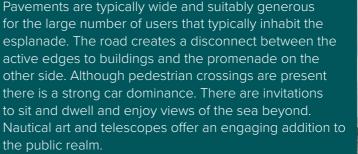
The scale varies from a 4-storey hotel to 3-storey semi-detached housing to a 7-storey apartment block with ground floor commercial / retail units. Although the grand pavilion is only 2-storeys, its generous form and tall architectural features (clock tower) give it a commanding presence along the street. The single storey pavilions offer a modest, human-scale addition to the public realm.

The boundary treatment differs with each typology. A car park defines the edge to the hotel with a low wall to the street. Semi-detached dwellings have a short defensible front garden with low masonry walls and metal railings. Tall hedges have been planted in some instances to provide a greater degree of privacy. The Grand Pavilion and commercial / retail units open directly onto the public realm. Food and beverage facilities have a shallow defensible space to the front with a low masonry wall. Chairs and tables fill this space and when occupied, creates a lively and animated edge to the street.



The materiality appears to vary with the age of buildings along the Esplanade. Older structures consist of both exposed stone and rendered masonry. These heavy and robust materials are often complimented with fine metalwork balconies and canopies. More contemporary structures, such as the large apartment building, colloquially referred to as the 'bottle-bank' consists of a large green copper façade. Recent structures, such as the single storey café extension on the promenade is made almost entirely of glass and permits uninterrupted views of the sea for occupants within.







3.2 CONNECTIVITY

Railway

The nearest train station to the site is in Pyle, which sits on the main line between Swansea and London Paddington. This provides connections to the wider region, including Cardiff and Bristol. From Porthcawl, the train station is accessible via approximately a 20min bus trip or by car.

Buses

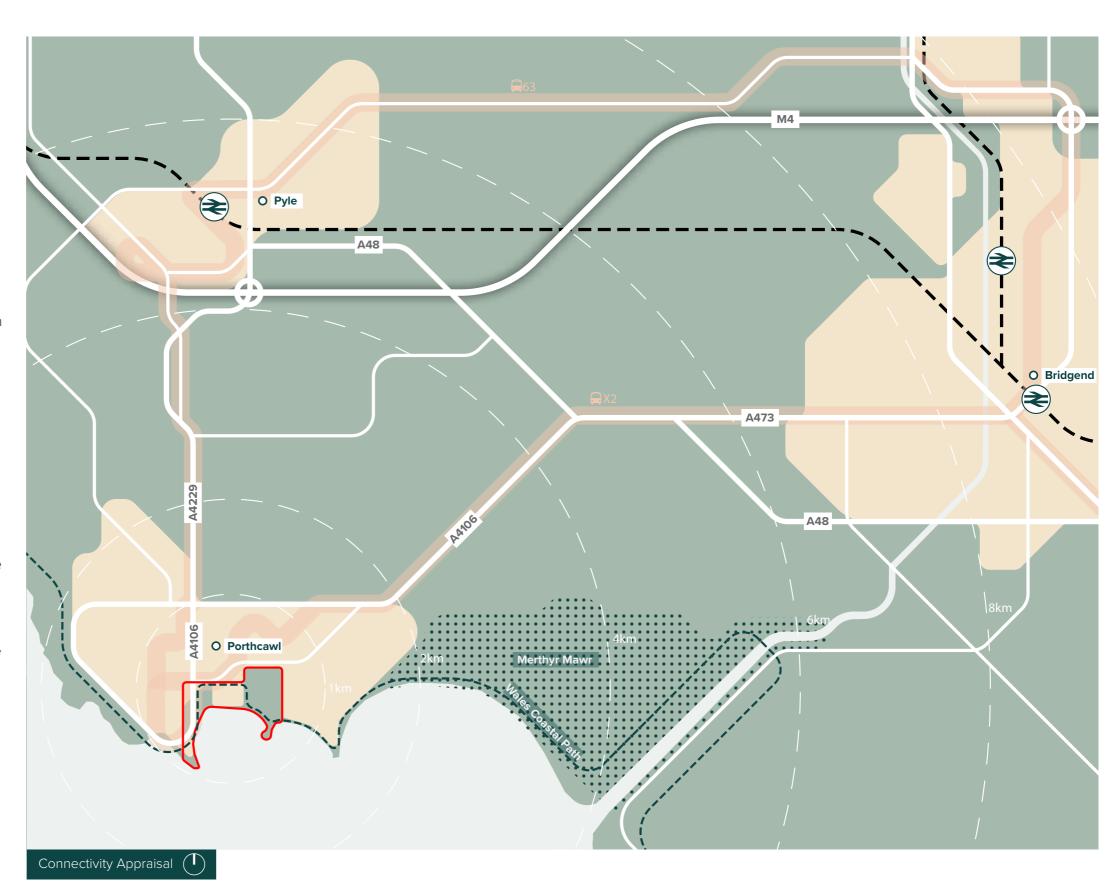
The town is served by two main bus services - 63 and X2. The former serves connections to Bridgend via Pyle Train Station with 3 hourly services, while the latter provides an express service to Cardiff via Bridgend with two services every hour. A new Bus Metrolink has been recently built within the site, which serves as a main connection point for these services. Additional / future services are anticipated to utilise the recent Bus Metrolink in parallel to the delivery of the proposals set out within this application.

Walking

The Wales Coastal Path runs via the site, connecting it to a wider footpath network along the coast. This includes connections east towards Merthyr Mawr, which provides natural recreational opportunities within a close proximity. The path currently gets diverted around Coney Beach with existing legibility issues, which creates an opportunity to improve these links and to make the route more direct and legible.

Road Hierarchy

The site has direct vehicular access to the A4106 (The Portway), which provides further onward connections to Pyle and Bridgend via the A4229 and A48/A473 respectively. These routes adjoin junctions 36 and 37 on the M4 with further onward travel to Swansea, Cardiff and other key settlements along the corridor. It is anticipated that the existing site access A4106 access will be retained for future proposals.



Site Movement Appraisal

The Movement and Access Appraisal Plan illustrates the current mobility conditions within the site and immediate context. The plan showcases vehicular and active travel patterns, identifying areas where vehicle dominance is prevalent. Additionally, it highlights key connections and breaks between different key areas of Porthcawl. Key observations include:

- Areas of the site are heavily dominated by vehicles, overshadowing pedestrian access and creating unsafe conditions for walkers.
- There is insufficient cycle infrastructure, which could discourage cycling as a mode of transport.
- Active travel routes are insufficient, with a part of the Wales Coastal Path running along a busy and narrow road, New Road, posing safety risks.
- There are clear east-west connections from the Esplanade to Trecco Bay that could be improved to promote pedestrian and active travel routes.
- The A4106/The Portway and the large Salt Bay site create a major disconnect between Coney Beach and the town centre, hindering seamless movement.
- The Eastern Promenade is predominantly used for car parking, which limits its potential as a vibrant pedestrian-friendly space. This makes Eastern Promenade road unnecessary as a vehicular route.
- The pedestrian routes into the town centre are visually poor, detracting from the town's overall attractiveness.



3.3 HERITAGE

Pre 1900s

Porthcawl's origins trace back to the early 19th century, marked by the construction of its harbour in 1825. Built as a coal port, the harbour played a crucial role in facilitating the export of coal from the nearby Llynfi Valley.

During the mid to late 19th century, Porthcawl started to attract visitors looking for seaside holidays with the coming of the Great Western Railway (GWR), making it more accessible. This led to the growth of the town centre and many of the town's key buildings and infrastructure were built during this period, including hotels and guest houses, including the Marine Hotel and Marine Terrace in 1886 and the Esplanade and Esplanade Hotel in 1887.

Porthcawl's unusual Lighthouse made its appearance in 1866, shortly after the pier was extended and, although it was designed to improve facilities at Porthcawl Dock, it also signified the gradual shift towards ensuring greater levels of safety at sea throughout the 19th Century. It is generally acknowledged that James and Mary Caroline Brogden played a major part in Porthcawl's early development and actively promoted the town as a modern seaside resort which had been largely attained by the time Porthcawl Dock closed in 1906.

The closure of the Dock removed Porthcawl's industrial role. Large areas of the residential streets started to be constructed after 1907, north and east of the main high street. This was a network of terraced streets with houses that had slate roofs, bay windows with small front gardens. The exception was Westbourne Place which consisted of slate-roofed, flat-fronted, red-brick terraces with no bay windows and no front gardens.





Post 1900s

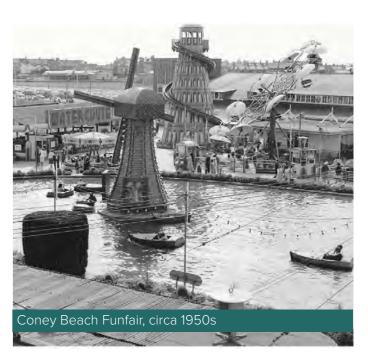
Although GWR had officially closed Porthcawl Dock in 1906, the railway was still intact although not fit for the movement of people. Due to this, there was a new railway station built in Dock Street in 1916 nearer the beach which served the Porthcawl community well until its demise in the 1960s.

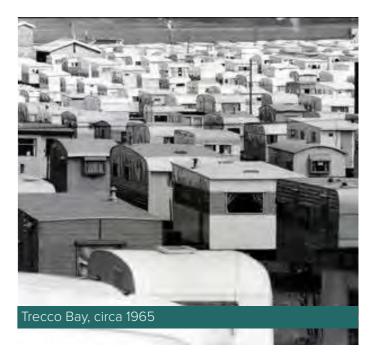
Finding its feet as a growing tourism seaside resort in the early 20th century, the town centre saw further development to cater to the increasing number of tourists with amenities such as the Grand Pavilion which opened in 1932 and John Street, which became the main pedestrianised retail high-street later in the 21st century.

During the inter-war 20th century, there was a desire for increased leisure and recreational facilities in Porthcawl. This included improvements to the Eastern Promenade and transforming allotments into Griffin Park with bowling and putting greens, tennis courts and a spacious children's playground that the majority still exist today.

The inter-war period also brought development to Salt Lake as it became a popular amenity space in Porthcawl, contrasting with the earlier dock era. Around 1926, the old Inner Dock Basin was bricked up by Porthcawl Urban District Council, creating a lagoon as part of the Eastern Promenade Development Plan. This lagoon, known as Salt Lake, provided new recreational facilities, including swimming and boating, and featured a miniature light railway from the early 1930s to the 1950s, delighting many generations.

By the late 20th century, Trecco Bay had evolved into a full-fledged holiday resort with upgraded facilities, including static caravans and extensive leisure amenities.





Porthcawl 1884



In the mid to late 19th century, Porthcawl began attracting tourists seeking seaside holidays due to the arrival of the Great Western Railway, which made the town more accessible. This increased tourism spurred the development of the town centre, leading to the construction of key buildings and infrastructure.

Porthcawl 1900



As a growing seaside tourism seaside resort in the early 20th century, the town centre saw massive development, including the Grand Pavilion being built.

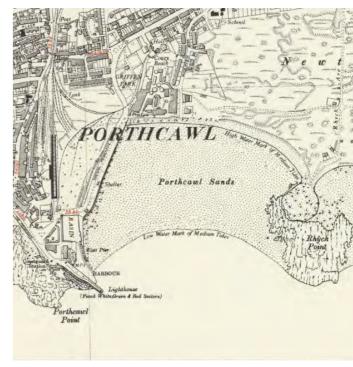
Improvements were also made to the Eastern Promenade and transforming allotments with bowling and putting greens at Griffin Park. The lagoon, known as Salt Lake, offered new recreational facilities with swimming, boating facilities and a popular miniature railway emerging.

Porthcawl 1921



Coney Beach, Porthcawl's funfair, opened in 1918. Porthcawl suddenly mushroomed into one of the most popular holiday resorts in the country. From 1948 it was the venue for the South Wales Miners' Eisteddfod. For many people, Coney Beach Amusement Park is the place that defines Porthcawl and that it reflects the narrative of Porthcawl's growth and development into a major 20th century coastal destination.

Porthcawl 1949



With the onset of WWII, Salt Lake was re-purposed for the war effort. In 1943, it was filled in to accommodate forces needing space.

Trecco Bay started as a modest camping site in the mid-20th century. It expanded significantly in the 1960s and 1970s, becoming one of Europe's largest caravan parks due to the rising popularity of caravanning. Throughout the mid to late 20th century, Porthcawl continued to thrive as a holiday destination. In more recent years, Porthcawl's traditional seaside role has faced challenges due to changing holiday habits and economic conditions.

Porthcawl Waterfront Regeneration

Heritage Value

The town's heritage value is anchored by several significant listed and locally notable assets. Key statutory-listed structures include the Grand Pavilion, an Art-Deco ferro-concrete seafront theatre dating to 1931–32 (Grade II), the early-19th-century Outer Basin walls and West Pier, integral to Porthcawl's maritime history (Grade II) and the modest yet historically rich Public Conveniences on the main shopping street (Grade II).

There are a number of Grade I listed buildings surrounding the site. The medieval St John the Baptist Church, founded in the 1180s, holds exceptional architectural and historical importance (Grade I). Newton Primary School (Grade I) which borders the site, designed in a Queen Anne style by D. Pugh Jones and built in the early 20th century, it survives largely intact.



LEGEND



Listed Buildings and Conservation Area Plan

3.4 TOPOGRAPHY

A topographic survey of the site has been undertaken. The topography of the site is complex with a maximum difference of height across the whole site of up to 10.58m.

There are areas within the site where level changes are more significant. Key areas include:

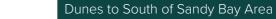
- Pedestrian walkway along Sandy Bay beach front, along Mackworth Rd, and notably where the emergency access meets the beach.
- The levels between Hillsboro Car park, Salt lake Car park and Eastern Promenade, a depression is present where the former dock has been filled in.
- The significant level change at Sandy Bay Bowl (a significant depression) and Monster Park, which is one of the highest points of the site.
- The levels increase towards the north of the Sandy Bay area owing to a mound understood to have been created as a result of the excavation of the Sandy Bay Bowl.
- The dunes sit notably higher than Sandy Bay to the north.
- The Hi Tide and Coney beach area is relatively flat owing to existing development with a retaining structure to the eastern edge which somewhat defines the boundary of Griffin Park.



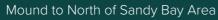














Retaining Structure at Griffin Park

3.5 FLOOD RISK

NRW Mapping

The flooding situation and existing drainage network in Porthcawl have been assessed using data from Natural Resources Wales, alongside topographical mapping to identify low-lying areas and potential locations for Sustainable Drainage Systems (SuDS) within the scheme.

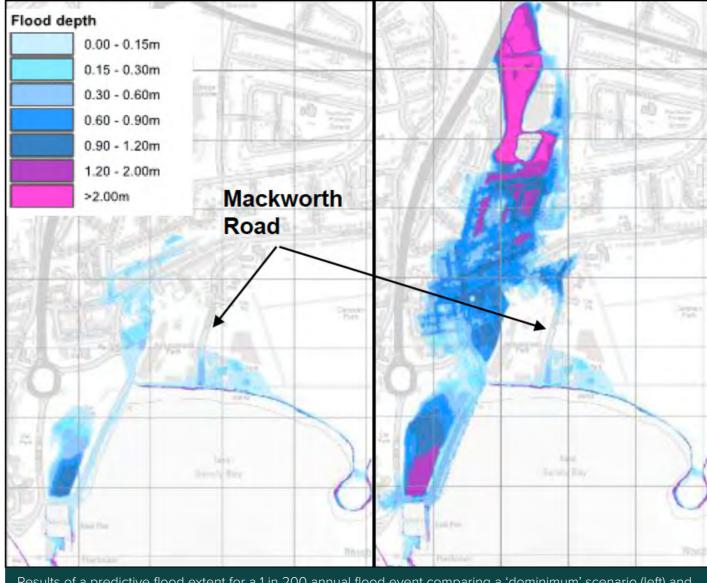
The areas identified as having the highest flood risk are located around Cosy Corner and the harbour. Medium flood risk zones include the beach areas along Coney Beach, the Hi Tide area, and extending down towards the lighthouse. In contrast, lower flood risks have been identified in the Salt Lake area and some of the surrounding residential neighbourhoods.

Coastal Flooding

The crest levels of the private defence structures along the Coney Beach Fairground are not adequate in places. However the extent of predicted flooding only extends about 100m northwards along Mackworth Road. Flooding is limited to adjacent properties at the southern end of Mackworth Road. During a 1 in 200 year event five businesses are at risk of flooding. This increases to 18 businesses and 16 residential properties were the Western Breakwater to fail (Figure 5). With sea levels expected to rise by an assumed 1.11m between 2020 and 2120, flood risk to the area will increase.

Mackworth Road along the Coney Beach frontage is a low spot at risk of wave overtopping and extreme tidal levels. Previous modelling suggested a pathway north to the New Road flood cell. However OBC modelling (FCERM climate change allowances as of January 2017, now superseded) found flood water is limited from running northwards by a highpoint in the road.

The Phase 1 area has been defined using the 1-in-200-year flood event for 2118, based on the more precautionary FCA climate change allowances (CL-03-16). In this scenario, sea levels are high enough for water to flow north from Coney Beach along Mackworth Road for around 400 m, reaching as far as Glan Road.



Results of a predictive flood extent for a 1 in 200 annual flood event comparing a 'dominimum' scenario (left) and in the 'do-nothing' scenario without the Western Breakwater (right). Image taken from the OBC (Arup, 2017).

Design and Access Statement

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3.6 ECOLOGY AND BIODIVERSITY

Habitats

The habitats within the Porthcawl Waterfront site are diverse but overall of low to moderate ecological value. The main habitat types recorded include modified grassland, other neutral (maritime) grassland, mixed woodland, urban trees, ornamental and non-native hedgerows, mixed and bramble scrub, introduced shrub, ponds (non-priority), coastal sand dunes, bare sand, low-energy littoral rock, and developed or artificial surfaces.

Grassland

Modified grassland occurs widely across the site, typically on sandy soils, and is intensively managed or disturbed by recreation and trampling. It is generally species-poor, dominated by common grasses such as cock's-foot, rye-grass, and Yorkshire fog, with typical amenity herbs. The other neutral grassland (notably around the Sandy Bay Bowl and Rhych Point) becomes more species-rich closer to the dunes, supporting species such as red fescue, bird's-foot trefoil, and ribwort plantain, and is assessed to be of moderate condition.

Woodland

Woodland habitat is confined mainly to the periphery of the former Monster Park, consisting of a mix of native and non-native trees with a sparse understorey and scattered ornamental planting. Urban trees across the site (especially around Griffin Park and the Aldi development) are mostly non-native ornamental species in moderate to good condition.

Hedgerows

Hedgerows are limited to the Griffin Park area and are generally species-poor, comprising privet, sycamore, and pine. None meet the criteria for 'Important' hedgerows under the Hedgerow Regulations 1997. Scrub habitats, mainly bramble and mixed scrub, occur on mounds and site margins, with moderate condition overall.

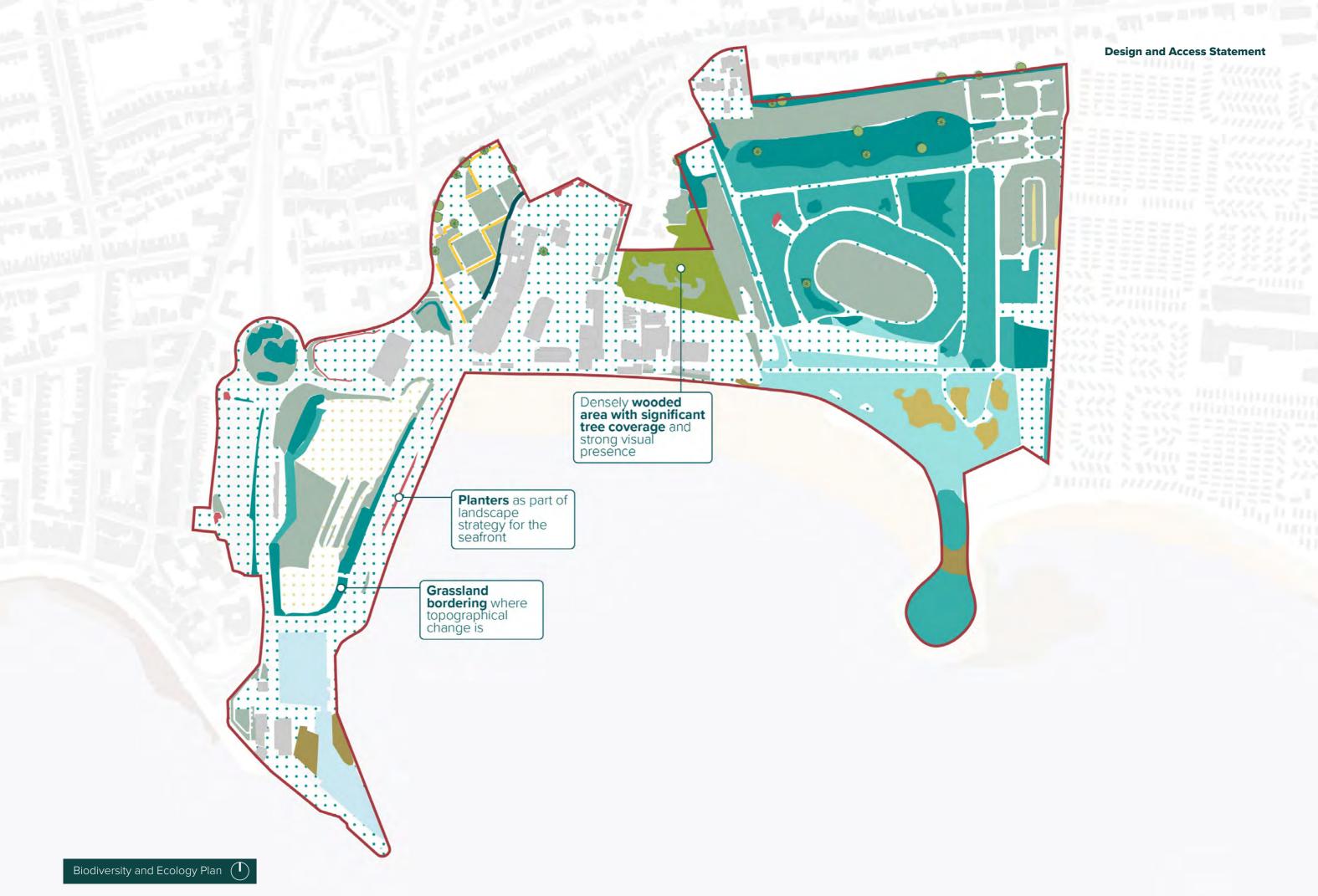
The only habitat of high ecological value is the coastal sand dune system, which qualifies as a Priority Habitat under Section 7 of the Environment (Wales) Act 2016 and is considered irreplaceable. The other habitats, including bare sand, ponds, introduced shrubs, littoral rock, and sealed surfaces, are of low intrinsic ecological value.

Reptiles

Faunal surveys identified small populations of common reptiles (common lizard and slow worm), moderate bat activity (particularly common and soprano pipistrelles, with occasional rare species such as Nathusius' pipistrelle and lesser horseshoe bat), and a moderate diversity of breeding and wintering birds, including several Section 7 Priority Species such as skylark and song thrush. Invasive non-native plants, including sea buckthorn and Japanese knotweed, were also recorded and are considered negative ecological features.

LEGEND



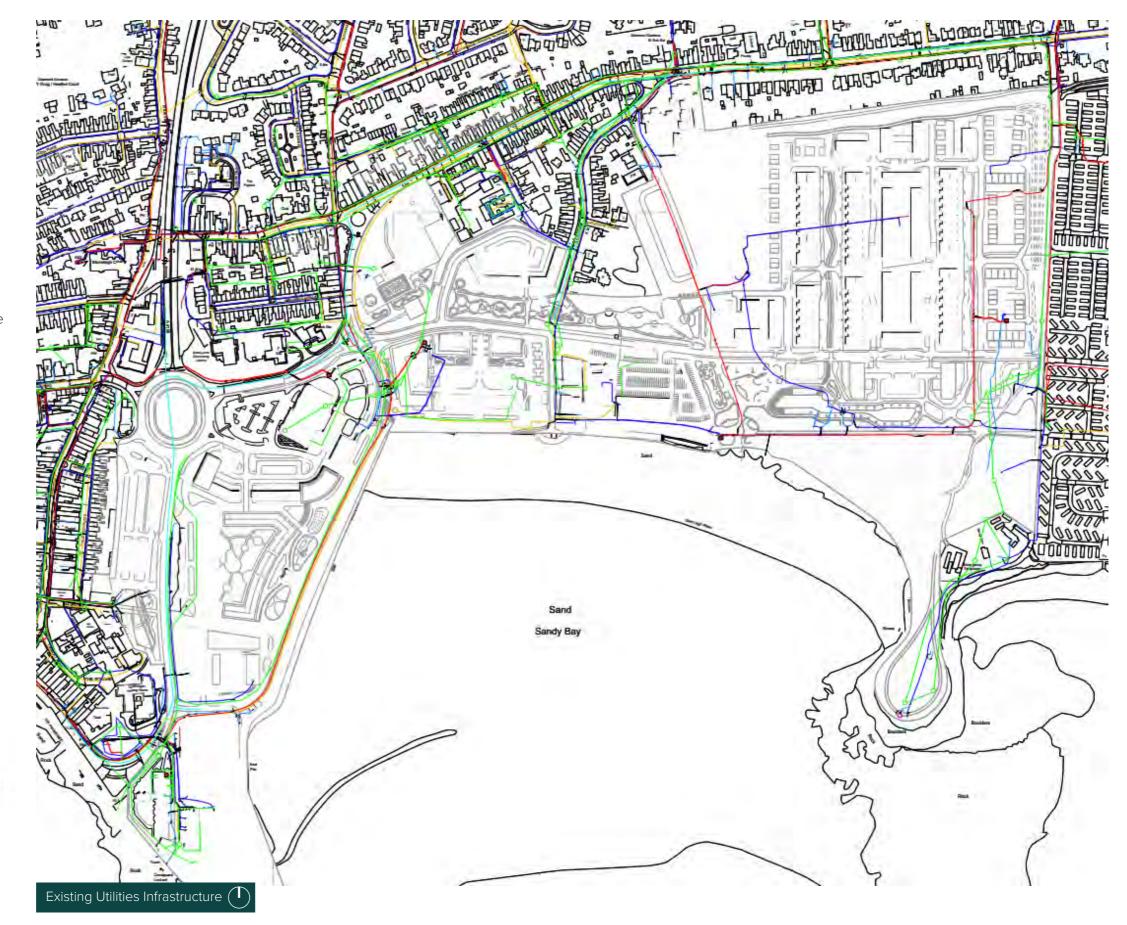


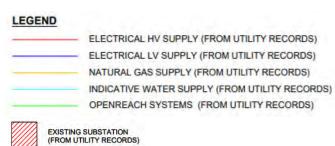
3.7 UTILITIES

Existing Utilities

An appraisal of the existing utilities infrastructure was undertaken to inform this application, providing an understanding of the current service provisions and any associated constraints that may influence development.

The findings are summarised within the Existing Utilities Plan, which identifies the location and capacity of key utilities and highlights any potential requirements for diversion, reinforcement, or protection. This information has directly informed the layout and design of the proposals illustrated within the Illustrative Masterplan.





3.8 NOISE

A baseline noise survey was undertaken in September 2025 to determine the existing sound climate at the Proposed Development Site.

The noise measurement locations were selected to be representative of the noise levels at the closest (and therefore worst-affected) locations of the noisesensitive receptors to existing sources.

The results of the baseline survey are presented within Volume 3, Appendix 8.2: Baseline Survey Report of the Environmental Statement.

Measured noise levels at the Site has been used as a basis to determine the extent of the required mitigation to meet best practice guidance and local policy requirements. Proposed mitigation measures are set out within Volume 1, Chapter 8: Noise of the Environmental Statement.



3.9 OPPORTUNITIES AND CONSTRAINTS

Landscape SWOT

There are a range of opportunities to enhance coastal landscape character areas and habitats and draw them into the site, creating green corridors to connect open spaces and a positive outlook for future visitors and residents.

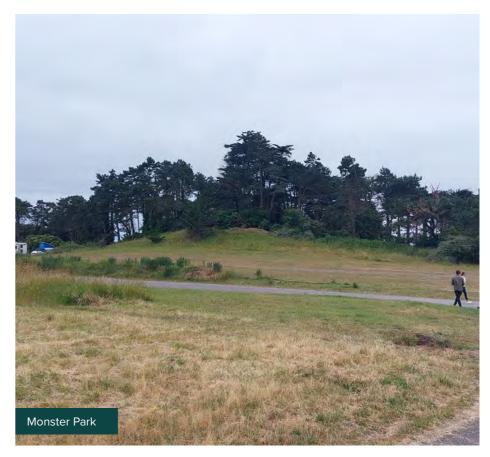
Primary landscape observations:

- Distinct dune habitat that requires protection and enhancement;
- The dune's character can easily be drawn into development with naturalistic fingers;
- Importance of the Sandy Bay Bowl space and extent of open grassland which is accessible to the community;
- Visual amenity and green impact of the trees within Monster Park;
- Opportunity to improve pedestrian connections to green space and nature;
- Views to the coast, from car park area and bus metro, also back to green space, Griffin Park as green anchor, opportunity for enhanced green corridor:
- Importance of Griffin Park as anchor to community;
- Opportunities to improve visual and physical green links through the site;
- Grassland and open space of Sandy Bay is an important area for residents and dog walkers as well as being a rich habitat. It will require mitigation in the form of provision of open space within the development;
- Opportunity for roundabout to be remodelled to provide improved open space at key gateway to the site; and,
- Opportunities to soften through landscape proposals the interface between the amusement park and Coney Beach.

Key Considerations

- Enhance rich coastal landscape areas and draw them into development to enhance health and wellbeing aspects of the masterplan.
- Use existing natural and green spaces to provide anchor points from which to enhance and expand to form connections and green corridors.
- Creation of new habitats and green infrastructure to provide and maximise multiple benefits for both people / nature and capture the strong local sentiment towards having strong access to green spaces.
- Create a new West to East green corridor to create an attractive active travel link through the site and link key open spaces such as Griffin Park, Monster Park and The Dunes. These spaces have the potential to play a vital role in the enhancement of outdoor recreational provision and reduce the pressures on Merthyr Mawr.







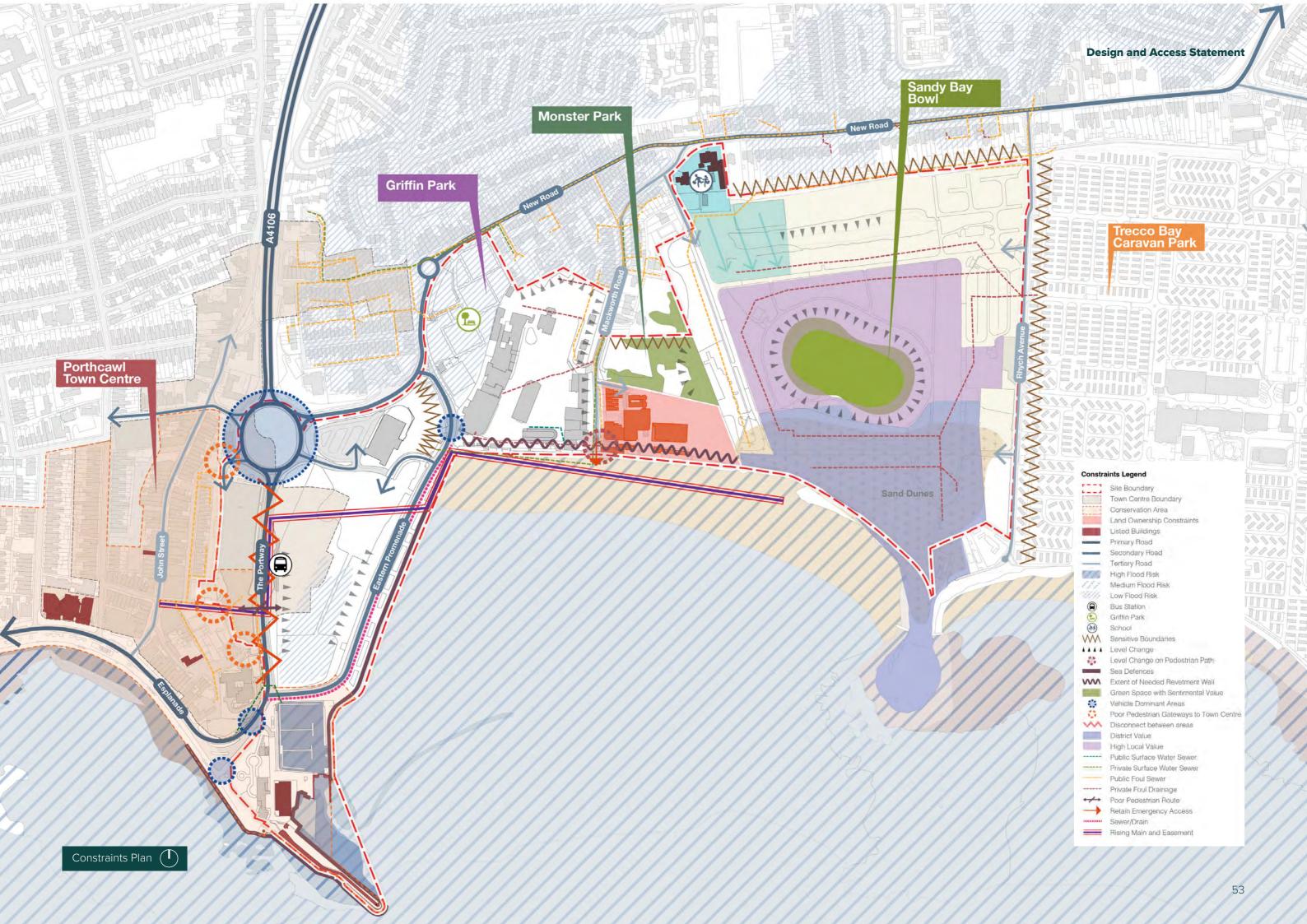
Constraints

The site constraints plan provides a comprehensive analysis of the critical factors impacting the development potential and design approach for the site.

Through analysis, we have identified several key constraints, including movement patterns and the standard of the links that establish them, topography changes across the site, disconnected areas, the quality of spaces and areas where boundaries and frontages require consideration.

Key Considerations

- The interface created by recently implemented new bus metro and immediate public realm.
- There are areas of level changes that pose accessibility and safety issues.
- Negative frontages have been identified which affect the overall streetscape.
- Pedestrian routes to the town centre are poor and lack sufficient wayfinding, preventing them from being recognised as proper pathways.
- The highway infrastructure in places creates area of vehicle-dominated environments that dissects the site, such as along The Portway creating an east-west disconnect.
- Ecologically rich habitats within the dunes and Sandy Bay area pose constraints and opportunities for the masterplan to respond to
- Land take associated with flood defence work proposals and evolution of the proposal within the placemaking strategy.
- Recent public realm works on Eastern
 Promenade which need to be responded to.
- Sensitive boundaries to surrounding context that require consideration to create a positive transition and interface, primarily Trecco Bay and the houses along New Road present challenges for access and aesthetic integration.
- Vehicle access into the site as a constrained parameter due to a combination of land ownership and existing traffic infrastructure factors. Vehicle access along the northern edge of the masterplan into Sandy Bay is particularly from the north and into Sandy Bay.



Opportunities

Porthcawl, with its scenic coastline and rich cultural heritage, presents a unique array of opportunities for development that harmonizes with its natural and built environment. The town's strategic location on the South Wales coast offers significant potential for revitalizing its waterfront, enhancing community amenities, and promoting sustainable economic growth. Through desktop and observational studies the following outlines the key site opportunities that can be leveraged to transform Porthcawl into a vibrant, resilient, and attractive destination for both residents and visitors.

Positive views and proximity to the sea creates a series of elements that shape potential opportunities to capture within the masterplan. Together, Salt Lake, Coney Beach and Sandy Bay provide key areas that are ideally suited for mixed-use developments that integrate residential, commercial, and recreational spaces. By incorporating rich coastal habitats into the design, these developments can enhance biodiversity, exciting and attractive places to visit and create a healthier living environment. The opportunities to provide multifunctional of green spaces, enhanced active travel links, and public amenities will not only improve the quality of life for local residents but also attract tourists and boost the local economy.

Moreover, the redevelopment of the Salt Lake area can consider reinvigorating the working maritime character and functionally of the town while also mending the physical and psychological disconnect between the town centre, Coney Beach, and Trecco Bay. If done well, potential exists to integrate modern infrastructure with these heritage assets in a manner that will ensure that Porthcawl retains its character while meeting the needs of a growing population. Capitalising on these site opportunities will allow Porthcawl to set a precedent for sustainable and inclusive coastal development

Key Considerations

- Enhance key views from along the promenade and within the site and town centre;
- Create connections between key destinations through the site such as the town centre, Coney Beach, Trecco Bay, with key pedestrian routes and enhanced gateways;
- Opportunity to increase the amount of Public Open Space (POS) from the placemaking strategy through the creation of a series of green corridors and spaces with varying roles and functions. There is potential to revitalise existing and create new spaces that will evenly distribute POS provision across the site and create a anchors to pull people through;
- Address wayfinding and natural 'nodes' by responding landmark points throughout the site with building heights and architectural forms that reflect their importance;
- Resolve sensitive edges along the perimeter of the site such as Trecco Bay and Sandy Bay, creating transitions and interfaces that enhance user experience and uplift the quality of the environment;
- Create more pedestrian orientated environments that respond to desire lines through the site, address vehicle / pedestrian conflict areas and place the pedestrian at the top of the place user hierarchy; and,
- Develop the water front in manner that capitalises on the positive views and proximity to the sea along the Eastern Promenade and along Coney Beach front.



Porthcawl Waterfront Regeneration

Opportunities and Constraints Photos

Several opportunities and constraints have been addressed through the previous plans. The images on the following pages give further detail of the qualitative nature of these aspects and show key opportunities or constraint within it's context.





2. Celebrate and capitalise on key views from the promenade and town







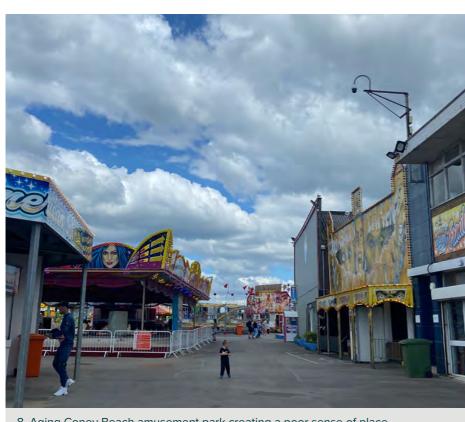






3. Enhance accessibility and connectivity green space within the site

4. Address key landmark spaces and movement corridors within the site



8. Aging Coney Beach amusement park creating a poor sense of place



9. Significant level changes along Coney Beach Promenade



10. Vehicle dominated environment along Eastern Promenade





4.1 DESIGN CONCEPT AND MASTERPLAN

Underpinning Design Approach

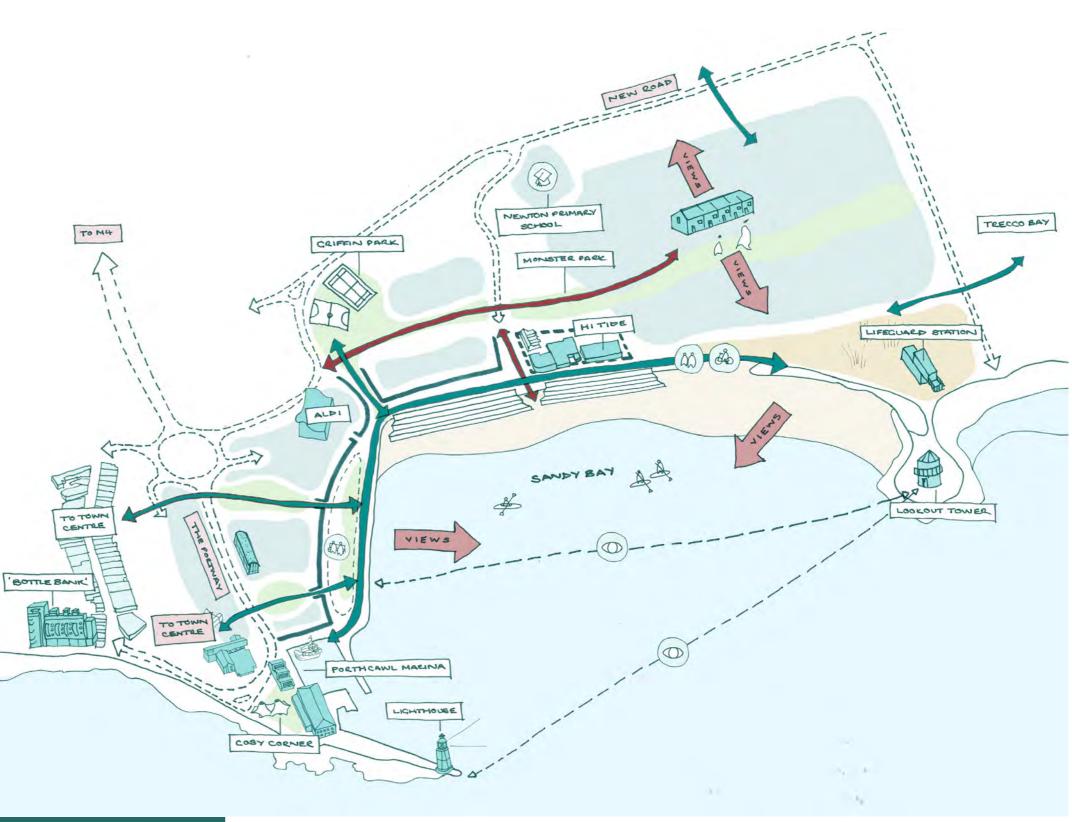
The underlying design concept for the Porthcawl Waterfront is rooted in creating a vibrant, resilient, and inclusive coastal community that celebrates the town's maritime identity while embracing contemporary placemaking principles. The scheme seeks to reconnect Porthcawl with its natural setting, including its beach, dunes, and coastal horizon, through an urban framework that prioritises accessibility, biodiversity, and social life.

By drawing the natural landscape into the heart of the development, the design concept integrates green infrastructure and adaptable public spaces that respond to the coastal environment, enhancing both ecological and community resilience.

At its core, the design is guided by the principles of the Placemaking Wales Charter, embedding six interrelated aspects into the spatial and architectural fabric of the scheme. Streets and spaces are designed to promote active travel, with pedestrian-friendly routes linking the town centre to the seafront and connecting key destinations such as Griffin Park, Sandy Bay, and the historic harbour. The public realm serves as the social backbone of the waterfront, offering spaces that encourage interaction, play, and cultural activity, while maintaining inclusivity and accessibility for all ages and abilities.

The principles of design quality, flexibility, and environmental sensitivity underpin the approach. Built form and landscape are designed to work together, creating a variety of sheltered spaces, integrating sustainable drainage systems, and ensuring that development harmonises with the town's scale and coastal character. Materials, planting, and detailing draw from the local vernacular and natural palette, reinforcing Porthcawl's identity as a distinct seaside town. Through a layered network of public spaces including the lively Promenade and Boardwalk the scheme aspires to create an environment where people, nature, and the built form coexist in harmony, shaping a sustainable and memorable waterfront for generations to come.

Key moves that stem from the design concept and principles for the waterfront are illustrated on the following page.



Regeneration Concept Illustration



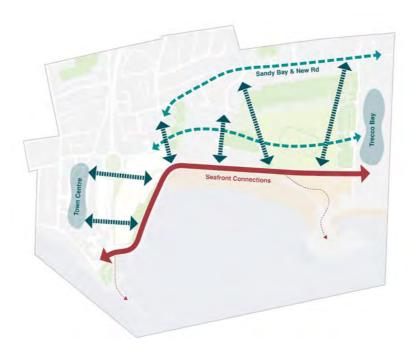
01 - Reuniting the town and the sea



04 - Designing a vibrant network of public spaces



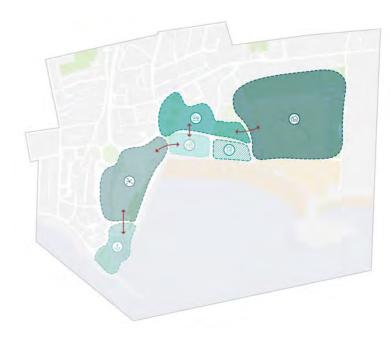
02 - Broadening offer and establishing distinct destinations



05 - Fostering better connectivity



03 - Designing for social connection



06 - Reflecting local character

Illustrative Masterplan Proposals

The following indicative layout demonstrates how up to 980 homes may be organised across the site together with a range of commercial, leisure and community facilities.

The design strategy is underpinned by a landscape-led approach that respects the coastal setting, prioritises placemaking, and creates a clear sense of identity and belonging. The development has been shaped by robust technical analysis, extensive engagement with stakeholders, and a commitment to delivering long-term value for the town and its environment. Key features of the proposals include:

- Up to 980 homes, (minimum 35% affordable with aspiration up to 50%);
- A mix of house types, sizes and arrangements to suit a variety of needs;
- Inclusion of community facilities and commercial facilities where viable;
- 2.2 ha of potential land allocated for educational use;
- Retention, expansion and enhancement of Sandy Bay Dunes;
- Creation of a new large linear park at the heart of the site via the retention, expansion and enhancement of Griffin Park, linking to a revitalised and accessible Monster Park;
- Creation of multifunctional landscape 'corridors' providing valuable recreation spaces and green outlooks for future residents;
- Extensive public open space provision incorporating equipped and informal play areas;
- Creation of a new pedestrian/cycle to realign the Wales Coast Path to follow a continuous route along the seafront;
- Utilisation of SUDS strategy fully integrated into place-making strategy; and,
- A contextually inspired and responsive approach to form and massing with particular regard to the Site's culture and history.

LEGEND

01: Porthcawl Lifeboat Station

02: Jennings Building (cafe / restaurant units and live work units)

03: Porthcawl Marina

04: Cosy Corner (small independent retail)

05: Harbourside Cafe

06: Block 01 - Hotel

07: Block 02 - Hotel and Tourist Information

08: Block 03 - Gym, Studio, Sauna, Bar/Restaurant and Seaside Apartments

09: Block 04 - Lido, Cafe, Lounge Seating, Undercroft Parking

10: Flexible Community Building/Space

11: Cycle Hub

12: Hillsboro South Pocket Park

13: The Harlequin Building (Zero Waste Shop)

14: Hillsboro Surface Level Car Park

15: Cycle Hub

16: Kiosk

17: Metrolink Bus Station

18: Block 05 - Convenience Store and Apartments

19: Block 06 - Apartments

20: Podium Space - Below Podium - Parking, Podium Level - Communal Garden Area

21: Block 07 - Small Retail Units, Cafe/Bar/Restaurant and Seaside Apartments

22: Public Lawn Space

23: Stepped edge

24: Cafe

25: Play area (Younger Groups)

26: Play area (Older Groups)

27: Flexible Interactive Fountains and Water Feature

28: Amphitheatre Space

29: Enhanced Gateway

30: Block 08 - Apartments

31: Block 09 - Cafe/Bar/Restaurant and Seaside

Apartments

32: Fun Fair

33: Performance Space

34: Aldi Store

35: Flexible Event Space (Seasonal Events, Market Stalls) 36: New Sports Courts (Including MUGA), Social Seating

and Informal Play

37: Enhanced Griffin Park

38: Block 12 - Cafe/Bar/Restaurant, Small Retail Space and Seaside Apartments

39: Block 13 - Specialist Retail and Seaside Apartments

40: Block 14 - Cafe/Bar/Restaurant, Specialist Retail and Seaside Apartments

41: Block 15 - Townhouses and Seaside Apartments

42: Public Boardwalk Route & Flexible Event Space (Market Stalls)

43: Pedestrian Links Between Sandy Bay Beach and New Public Car Park

44: Coastal Defence Terraced Revetment

45: Buccaneer

46: Hi Tide Inn

47: Hi Tide Inn Arcade (including gym development)

48: Hi Tide Inn Public Car Park

49: Private and Public Parking

50: Pump Track (All Wheel Facility)/Natural Play Area

51: Block 16 - Apartments

52: Block 17 - Apartments

53: Private and Public Parking

54: Large Formal Play Area

55: Woodland Walk

56: Landscape Area / Potential Motor Home Park

57: Mini Golf

58: Community Garden and School pickup/drop off

59: Sea and Beach Facility

60: Community Pavilion with Flexible Space, Formal Play

Area, Natural Amphitheatre and Play

61: Enhanced Dune habitat, Raised Boardwalks and

Activity / Viewing Platforms

62: Landscape Corridor

63: Trim / Fitness Trails and Outdoor Exercise Equipment

64: Land allocated for educational use

65: Newton Primary School

66: Seaside Apartments

67: Multifunctional Green Community Corridors

68: Sandy Bay Residential Area

*Note: Items bolded in the key are existing.



4.2 MOVEMENT FRAMEWORK

Underpinning Design Approach

The movement strategy seeks to sustainably connect the waterfront regeneration area with the wider context, benefiting new and existing residents. The strategy promotes active and sustainable travel options through safe and convenient access to public transport and pedestrian and cycle routes.

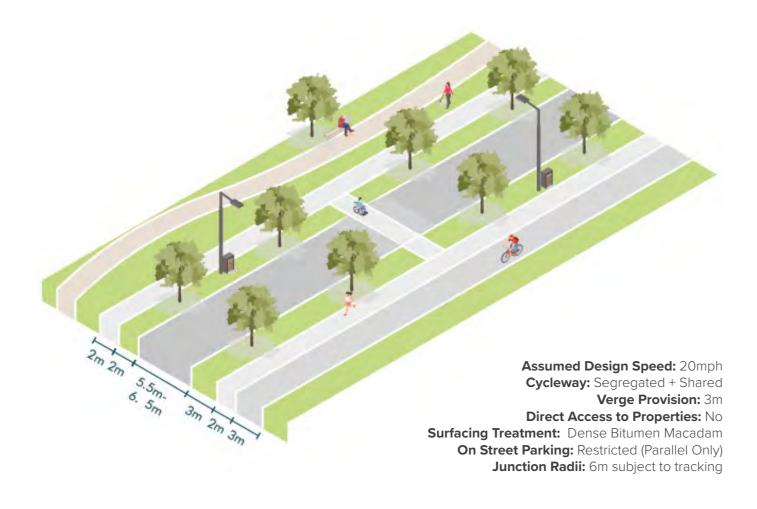
A new roundabout at New Road will provide a gateway to the Coney Beach and Sandy Bay character areas, closing off part of Mackworth Road and avoiding reliance on Sandy Lane or Rhych Avenue for vehicle access.

The proposal sets out a simple and highly legible street hierarchy. The primary streets will provide one of the primary means of access to Coney Beach and Sandy Bay character areas, connecting these areas to The Portway and wider network. Lower order secondary streets are concentrated within the Sandy Bay Village area and will serve individual homes. Tertiary streets are more liberally distributed within the wider site and provide access to key amenities including public car parks.

The proposals deliberately seek to restrict and limit the extent of vehicle movement within the public realm opting for more pedestrian focused and flexible environments capable of hosting events. Restricted access routes for emergency and maintenance vehicles form part of the proposals with these spaces.

The design specification and typical sections for the proposed hierarchy of streets are shown on the following pages.









Primary Street

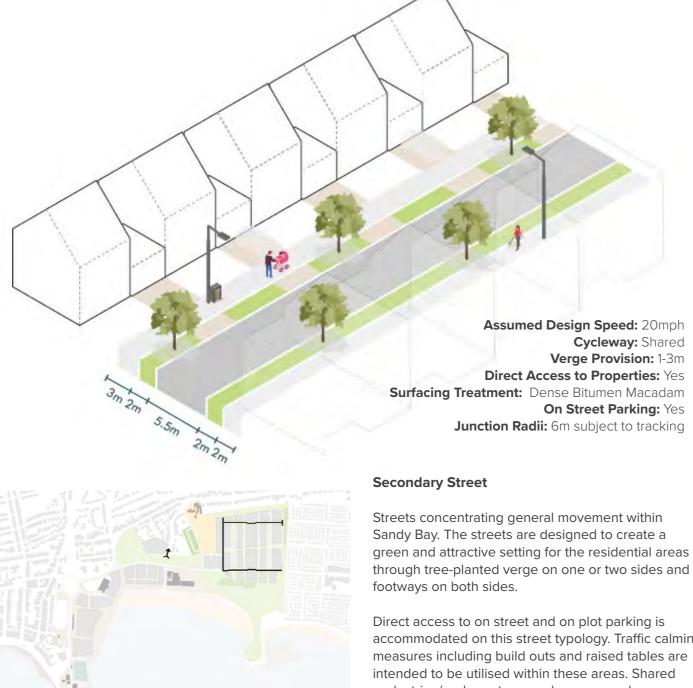
Street providing east-west movement between Sandy Bay and Coney Beach with Portway via a new junction on New Street. The Street intends to facilitate segregated pedestrian and cycle movements due to highest traffic flows and a double verge. No direct property access is required from this street type.

Where the primary street meets Sandy Bay Village the segregated pedestrian / cycle route will transition into a shared route with parallel parking spaces provided within the green verge sporadically.

Primary Street (Portway)

The existing Portway provides a key provision of north-south movement and general access to central Porthcawl.

The proposals do not intend to alter the carriageway widths, however the eastern footway and crossings will be improved to align with the localised proposals. This means that the footway and verge will be variable, however they will generally not fall below a 2m width.





Shared streets facilitate localised movement in Sandy Bay. Generally, proposed to be level, with pedestrian and cycle movements prioritised over vehicular movement. Subtle but clear delineation between vehicular and pedestrian spaces for visually impaired users. Streets will have very low traffic numbers so no designated cycle infrastructure will be necessary. Opportunities for greening is sought where possible to soften the general streetscape.

intended to be utilised within these areas. Shared pedestrian/cycle routes are also proposed on one side of the street which aligns with the anticipated



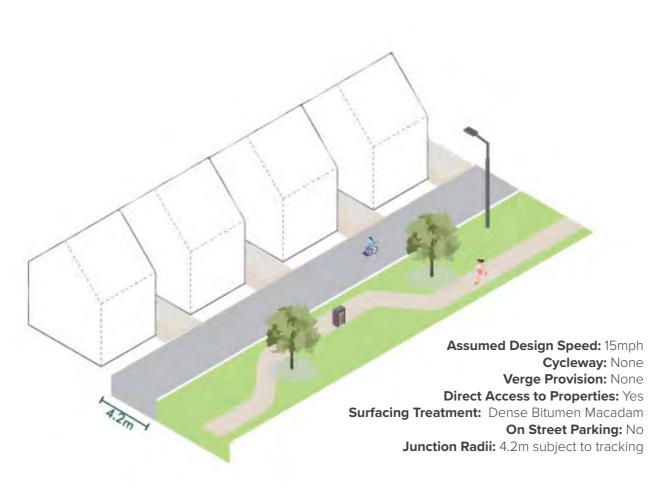


Assumed Design Speed: 20mph
Cycleway: On-Street
Verge Provision: None
Direct Access to Properties: Yes
Surfacing Treatment: Dense Bitumen Macadam
On Street Parking: Yes
Junction Radii: 4.2-6m subject to tracking

Tertiary Street - Access

Streets providing general access to apartment block interiors and parking areas. A simple carriageway and one or two footways will be sufficient due to generally sitting out of the public view.

Shared streets facilitating localised movement in Sandy Bay, particularly along green corridors to reduce the amount of hardstanding. Generally, proposed to be level with pedestrian and cycle movements prioritised over vehicular movement. Streets will have very low traffic numbers so no designated cycle infrastructure will be necessary. The adjacent green spaces will generally maintain a green and attractive character. One way vehicle movements are proposed to serve homes fronting these street and enable the delivery of an uninterrupted linear green corridors that can perform recreational and active travel functionality.





Private Drive

Single sided streets located around the periphery of Sandy Bay, serving under 6 dwellings. The streets are intended to facilitate slow speeds and no through movement. Private drives are not anticipated to be adopted by the Highways Authority.

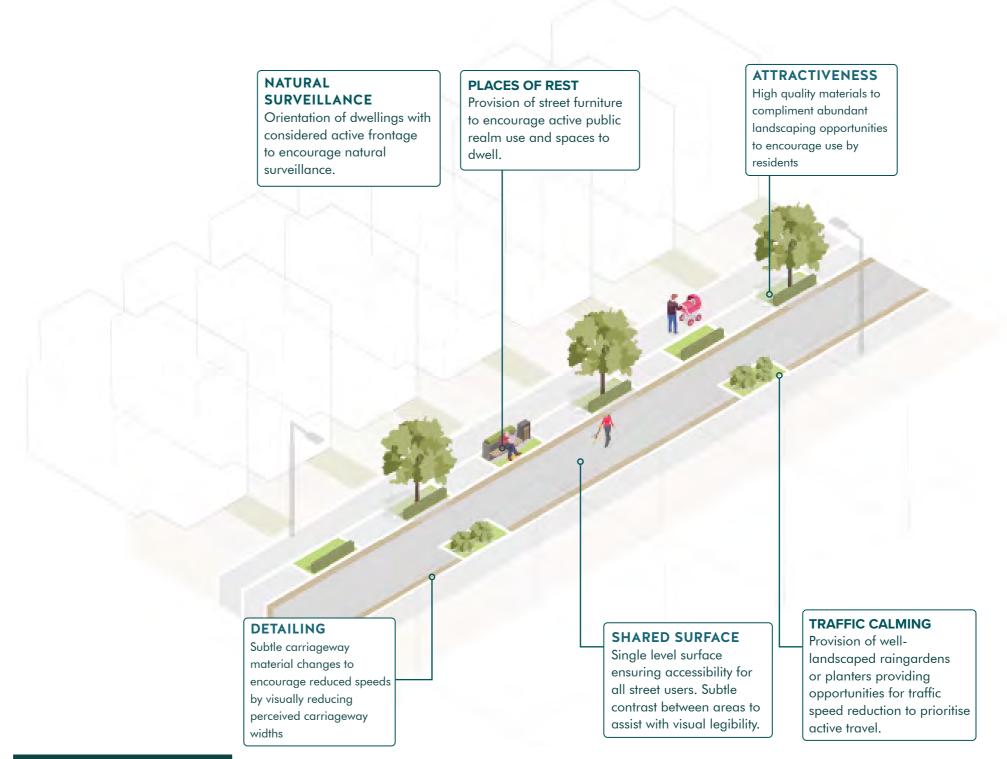


Traffic Calming

The development will utilise good design practice to promote active travel and healthy lifestyles. This will include encouraging residents to walk and cycle in attractive and safe environments that de-prioritises vehicular traffic.

The proposed streetscapes will generally accommodate lower traffic speeds, controlled through landscaping opportunities and material detailing. Coupled with passive and active opportunities for natural surveillance, this will ensure that streets are safe and overlooked.

In addition, ample opportunities for landscaping will increase aesthetic value of streets as well as provide environmental benefits, such as shade and cooling.



Traffic calming measures utilised

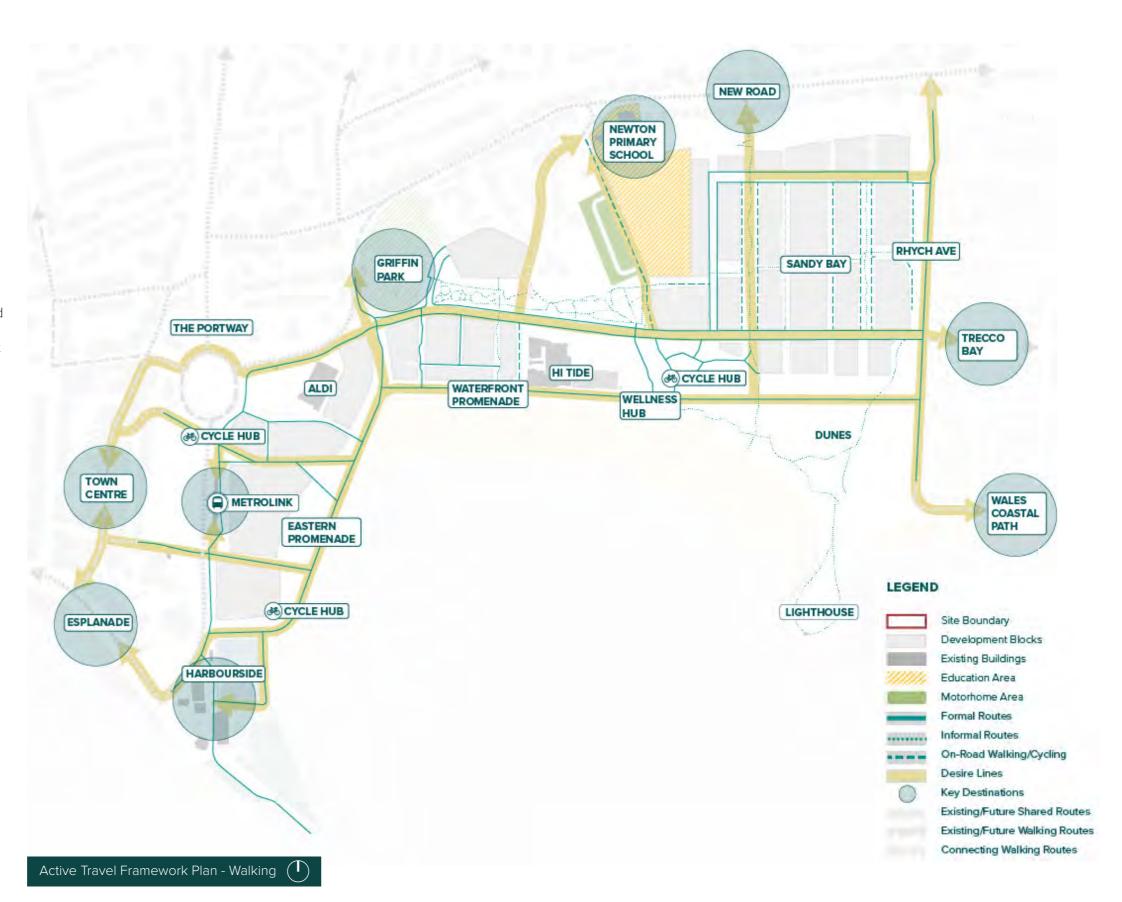
Porthcawl Waterfront Regeneration

Active Travel Framework - Walking

The development will be focused around integrating the new communities with the existing services and destinations in the area, which include Trecco Bay, Town Centre, Harbourside and various other leisure provisions. Key desire lines between these destinations will form the basis of the active travel framework. These connections will also link in to the wider Porthcawl LCWIP network.

A network of formal and informal walking routes will be established based around movement volumes and local constraints, such as ecology. Generally, formal routes will form the basis of most essential movement patterns and include paved and lit surfaces.

Informal routes on the other hand will provide a network of leisure routes that will have lower specification and management requirements.



Active Travel Framework - Cycling

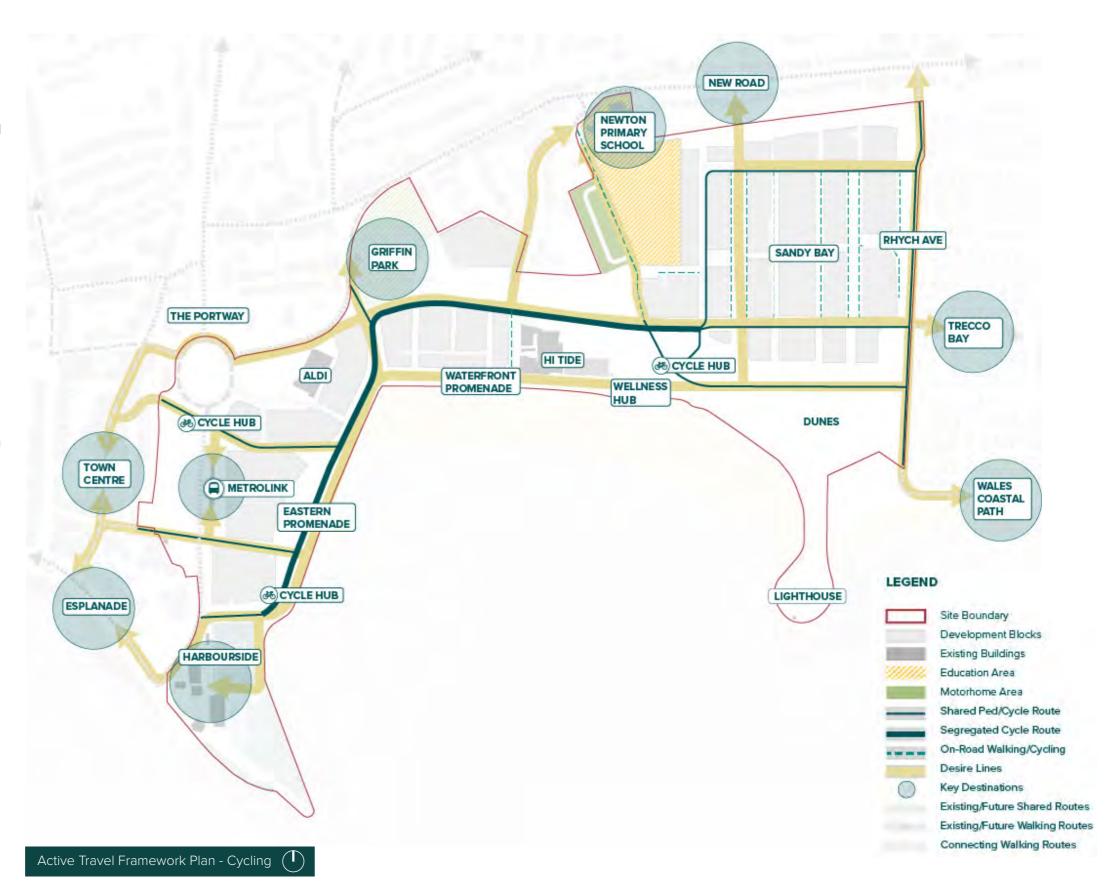
In addition to the walking routes, the development will also facilitate a network of cycle routes that similarly connect key destinations and services via the site as well as the LCWIP route network.

These will be also established within a hierarchy based on cycling and vehicular movement volumes and key desire lines with resulting segregation or integration of different users.

A segregated cycle route will be provided linking Trecco Bay with the Harbourside. This will partially be located along a Primary Street and pedestrianised Promenade, however generally cyclists will be segregated due to higher movement volumes.

This will be supported by radial shared routes linking cyclists to key destinations. These will be facilitated alongside pedestrians and may be off-road or within a pedestrianised promenade area.

Lastly, where traffic volumes will be lowest within residential areas, on-street cycling will be permitted within attractive shared spaces.



4.3 LANDSCAPE AND PUBLIC SPACE

Multifunctional Spaces

The development proposals have been designed to deliver a playable, green, and multifunctional landscape and public realm that supports healthy, active lifestyles, social interaction, and community well-being. Nature and green infrastructure are central to the design, woven throughout the site to provide ecological, social, and environmental benefits while contributing to the character and identity of the scheme.

Play and Recreation

Play will be a defining characteristic of the public realm, integrated across formal parks, semi-public spaces, streets, squares, and green corridors.

Alongside traditional playgrounds, opportunities for nature play, play-on-the-way routes, and multifunctional playable elements, including landscape features, artworks, and integrated seating, will encourage active movement and exploration for all ages. The design follows local and national guidance, ensuring all play provision is inclusive, accessible, and responsive to community needs.

Key Open Spaces

Three primary spaces will play a central role in defining the character and identity of the development, acting as public gathering places, connecting key areas along the waterfront, enhancing accessibility, and supporting leisure, events, and the local economy. These include:

Salt Lake Promenade:

 A vibrant waterfront route integrating green corridors, play-on-the-way elements, and public realm improvements, culminating in a square or plaza fronting the historic Harbourside.

Coney Beach Boardwalk:

 Linking coastal habitats with recreation spaces, informal play, and active travel routes.

Sandy Bay Gardens and Coney Beach Park

 Multifunctional green spaces providing biodiversity, planting, and areas for social interaction, recreation, and events.

A series of design principles and indicative uses have been proposed for each key space, providing guidance for detailed design while remaining flexible to respond to community needs and site context.

Green Infrastructure and Biodiversity

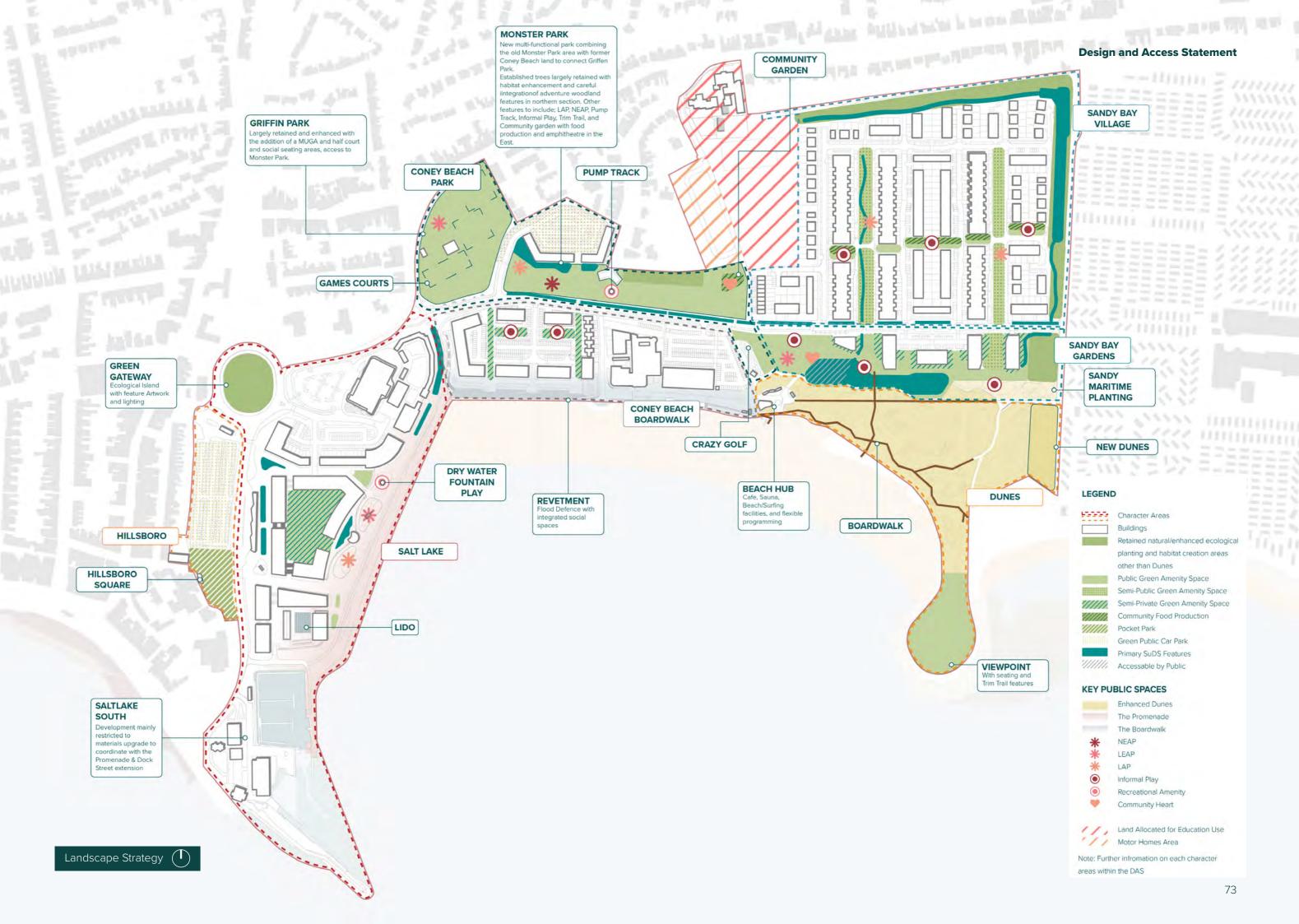
The landscape is structured around a hierarchy of green spaces that connects existing natural areas with new habitats and multifunctional open spaces, delivering biodiversity enhancement, ecological connectivity, and a high-quality public realm. This hierarchy includes:

- Natural Spaces: Existing habitats will be enhanced, with new habitats proposed to connect them and improve ecological access.
- Parks and Formal Green Spaces: Griffin Park will be enhanced and extended into a larger linear park
- Semi-Public Spaces: Expanded school grounds will provide green areas for sports, play, and informal recreation.
- Squares, Village Greens, and Pocket Parks: Local spaces for play, social interaction, and community gatherings.
- Streets and Green Corridors: Trees, planting, and SuDS features contribute to the wider green infrastructure network.
- Communal and Private Gardens: Residential gardens, allotments, and growing spaces will support biodiversity and well-being.

Integrating Nature Across the Site

Opportunities for green roofs, walls, balconies, and street planting will be explored to enhance biodiversity, connect habitats, and increase ecological and visual value. The overall strategy ensures that green spaces are accessible to all, creating a connected and multifunctional landscape that benefits both people and wildlife.

This approach establishes a resilient, climate-responsive, and ecologically rich public realm, where play, recreation, nature, and key civic spaces are seamlessly integrated to support a healthy, active, and socially vibrant community.

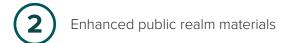


Salt Lake Harbour

Salt Lake Harbour will be a vibrant mixed-use waterfront destination celebrating the town's working heritage and coastal character and home to a new Lido. It will form a key anchor within the wider masterplan through providing a place where community life, local enterprise, and leisure come together throughout the year.

The public realm will feature a series of flexible spaces along the sea wall and harbour edge, designed to support outdoor dining, small markets, and informal gatherings. Together, these elements will create a sociable, inclusive environment that celebrates the harbour's working identity while enhancing access and enjoyment of the waterfront for everyone. Alongside the public realm enhancements, the proposals include repairing the existing crane to support the marina's operations.





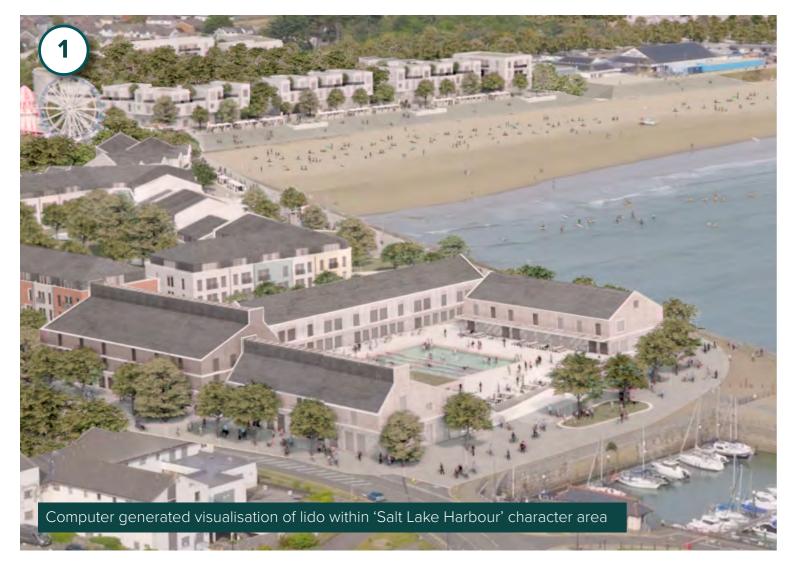


Feature lighting to illuminate old harbour

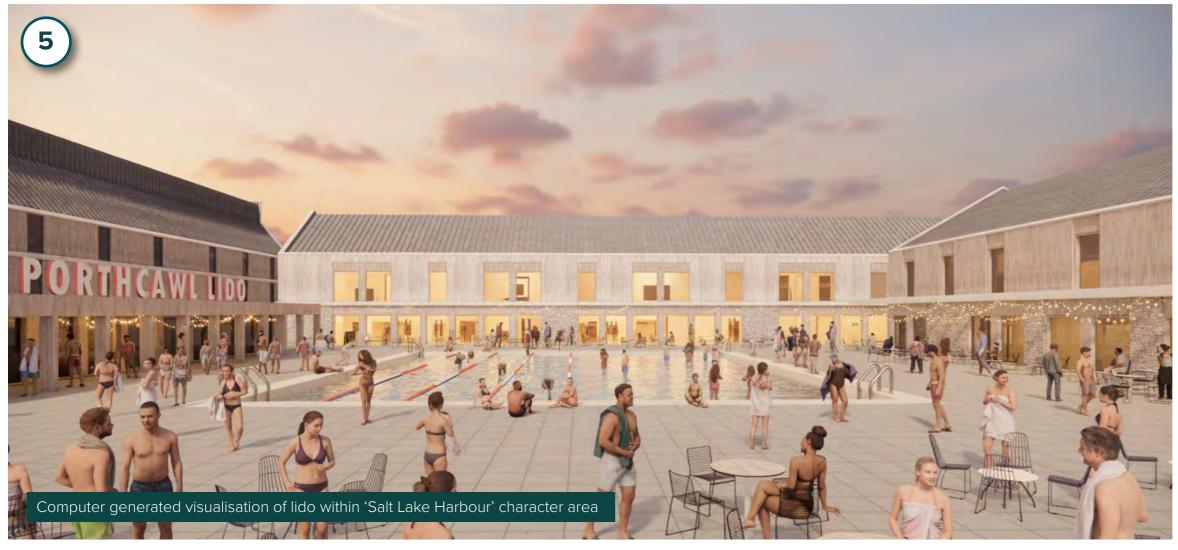
Programmed projection illumination on lighthouse

Upgraded promenade with SuDs, 6 landscaping and seating







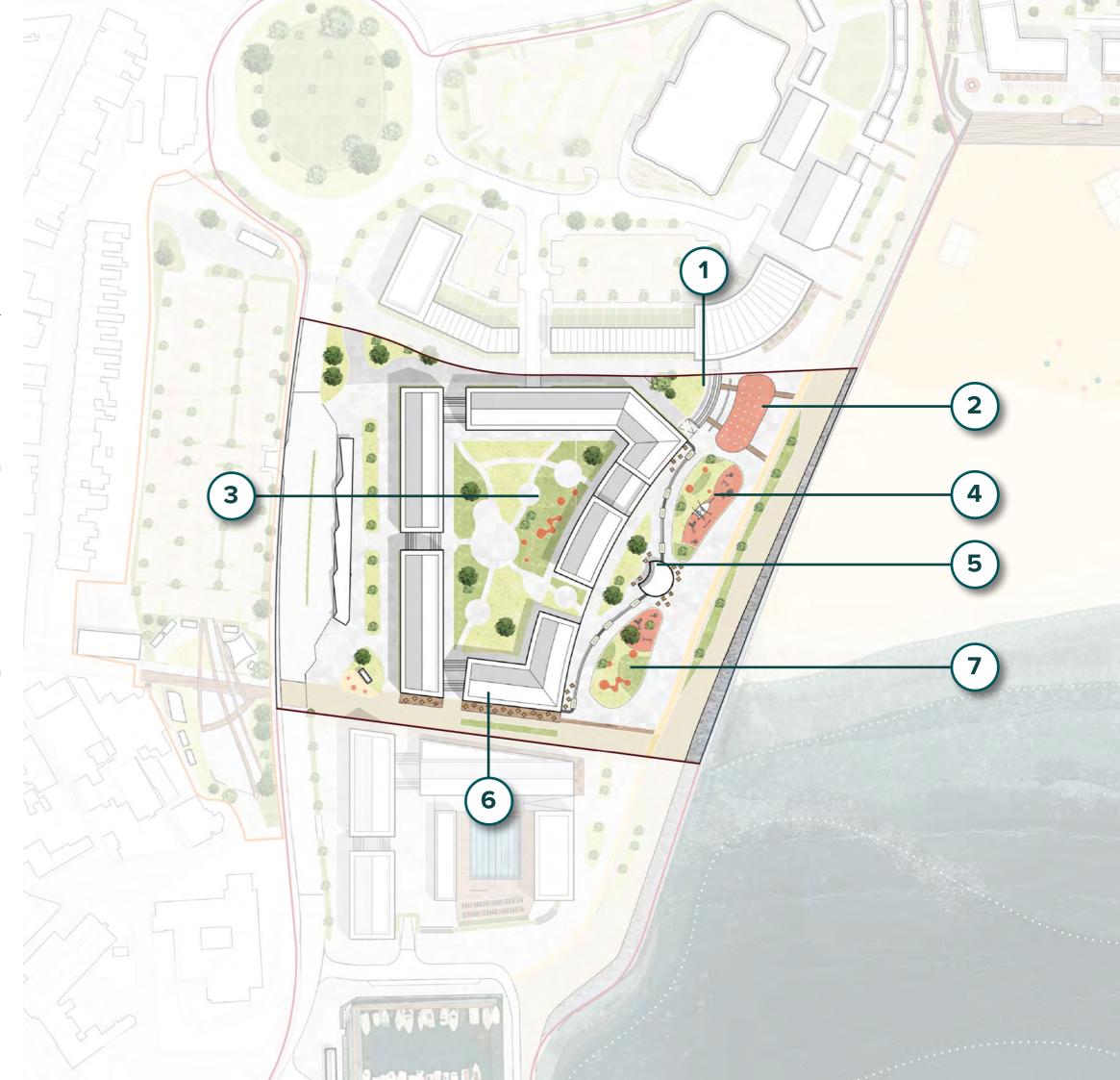


Salt Lake Promenade

Salt Lake Promenade is the main waterfront route linking the historic harbour with the town centre, linear park, and wider coastal development. Designed as a lively and inclusive public space, the Promenade will offer places to walk, rest, play, and gather accommodating everything from casual strolls to seasonal and community events.

A mix of seating areas, play elements, and outdoor dining spaces will activate the waterfront, while new trees, planting, and structures will provide shelter from wind and sun. The existing active travel route will be retained and enhanced with clear wayfinding and improved connections to surrounding destinations.

- Social green space with views across the bay.
- **2** Feature dry water fountain.
- Courtyard; multifunctional green space with amenity and SuDS.
- LEAP with sculptural boat themed play and integrated SuDS.
- Social street space with central feature structure
- 6 Commercial spill out with green street spine.
- 7 LAP with integrated SuDS





Computer generated visulisation of 'Salt Lake Promenade' including social green space with views across the sea





1.1.1 Salt Lake Crescent

Salt Lake Crescent

Salt Lake Crescent is a primary gateway to the wider masterplan and includes a number of proposals within the public realm design to support this role, ensuring a strong and positive sense of arrival.

Salt Lake Crescent proposals focus on creating a high-quality public realm that prioritises pedestrians and cyclists while limiting through-traffic. Proposals include enhanced landscaping and public art at The Portway roundabout, establishing a distinctive gateway and improving the visual identity of the area.

- Green gateway: Feature sculptural artwork with lighting, characteristic planting display including naturally colonised grassland based on redistributed sandy topsoil.
- Accessible ramp with integrated seating, steps and planters.
- Green SuDS street spine.
- 4 Commercial spill out.





Computer generated visulisation of northern portion of 'Salt Lake Promenade' including Fun Fair and beach





Coney Beach Boardwalk

Coney Beach Boardwalk is the principal pedestrian route along the seafront, connecting Sandy Bay to the Promenade. The Boardwalk incorporates flood defence measures that step down to the beach while providing elevated viewpoints out to sea and informal seating.

The space is designed to be inclusive and accessible to all ages and abilities, supporting active travel and social cohesion. While the Boardwalk will have a primarily hard landscaping character due to its exposed location, carefully positioned trees and planting will soften the environment and define the central activity zone.

The proposals set out a clear spatial framework where active frontages will face the Boardwalk, with spill-out space in front of buildings. A central strip will be able to accommodate seating, lighting, and planting, while maintaining unobstructed pedestrian routes on either side. View corridors will connect the coast to Coney Beach Park, with viewpoint areas located at breaks in the blocks.

1 Feature landmark way-finder

2 Stepped Revetment

3 Seating and viewing platform build outs

Promenade with commercial spill out areas and green spine

Semi-private garden areas with play and SuDS









Coney Beach Park (extended Griffin Park)

Coney Beach Park is a large new central green corridor that links Griffin Park, the former Monster Park, and the proposed Sandy Bay Gardens, extending through to the Relic Dunes. The park will provide a mix of recreational and active spaces for the community and visitors alike including a new pump track and NEAP.

Integrated play will be delivered within this location combining formal and natural woodland play. Health and wellbeing will be supported through outdoor exercise equipment, trim trail and a proposed wheel park with a pump track.

Coney Beach Park also forms a key movement corridor, supporting an active travel network through the green space. New homes and a public car park are proposed along the northern edge, integrating residential and recreational uses while maintaining the park's accessibility and connectivity.



- Multi-functional park combining old
 Monster Park area with Coney Beach land
 to connect Griffin Park
- 3 Wheel park and pump track
- 4 Woodland board-walk
- **(5)** Community garden
- 6 Crazy Golf











Sandy Bay Village

Sandy Bay Village is a coastal settlement characterised by its proximity to the beach and dunes, with native forest style planting integrated into sandy landscapes. The village will feature low-rise, high-density clusters of housing set within generous shared landscapes, complemented by modest private spaces such as small courtyards and balconies.

Community spaces will be designed to encourage interaction, incorporating features such as outdoor seating, play areas, and potager gardens. The architecture will draw on Porthcawl's heritage, with simple, elegant, and enduring buildings with 'outdoor living' elements including porches and balconies.

Clusters of dwellings will frame shared streets radiating from a central sandy village green, providing views across the dunes, inland hills, and the sea. Parking will be discreetly integrated, and road layouts will prioritise pedestrians with low-speed limits, creating a safe and sociable village environment.

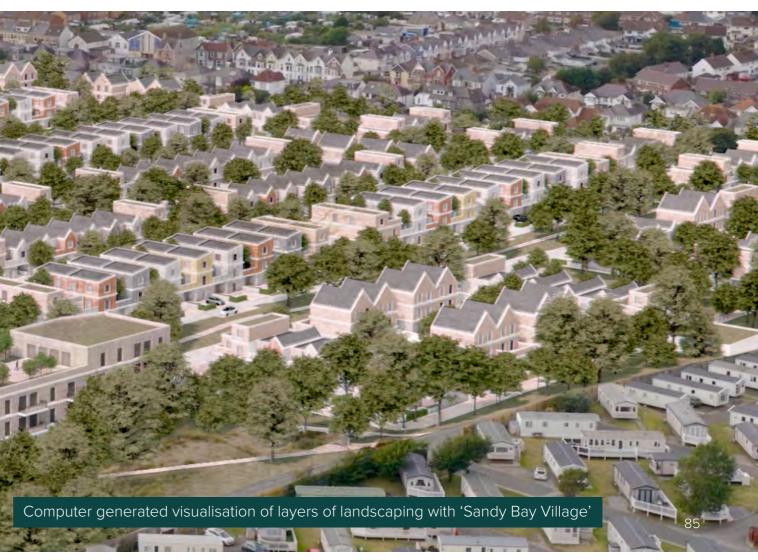
- Woodland and Scrub buffer with naturally cultivated site procured sandy topsoils.
- SuDS basin with conveyance swales in the north and south.
- Small Community Orchard areas with Informal play
- 4 Local Area of Play (LAP).











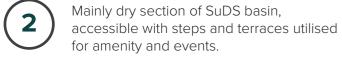
Sandy Bay Gardens

Sandy Bay Gardens is a significant naturalistic open space located to the south of Sandy Bay Village, forming a key part of the green corridor between Coney Beach Park and the Relic Dunes. The Gardens will feature sandy landscapes and planting that create spaces for socialising, informal play, recreation, and shaded seating.

The proposals includes both formal and natural play areas and flexible areas for activities such as yoga, supporting health and wellbeing. Sustainable drainage will be integrated throughout, enhancing the environmental performance of the space. Multifunctional gabion style retaining structures will define the northern edges of the dunes, controlling their migratory nature as well as provide seating overlooking a primary public open space to the north.



LEAP



Flexible amenity space and viewing platform.



Existing dunes retained with gabions with integrated seating.



Semi public garden space associated with apartments.



Play areas.



Sandy coastal style planting utilising site procured sandy topsoils and allowed to naturally cultivate.









The Dunes

The dunes area will create a cohesive and accessible transition between the village, parklands, and beach. Raised boardwalks will form network of routes within this public realm, guiding movement through the currently fragmented dune environment while supporting active travel for pedestrians and cyclists.

The boardwalks will provide elevated viewpoints across the dunes and coastline, offering opportunities for rest, observation, and connection with nature while also unifying the existing sand and vegetation.



Beach activity node with surf and wash facilities, lockers, sauna, F&B, and flexible event space.



Existing Dunes retained with gabion structures with integrated seating.



Existing Macadam path removed and replaced with boardwalk allowing sand movement beneath.



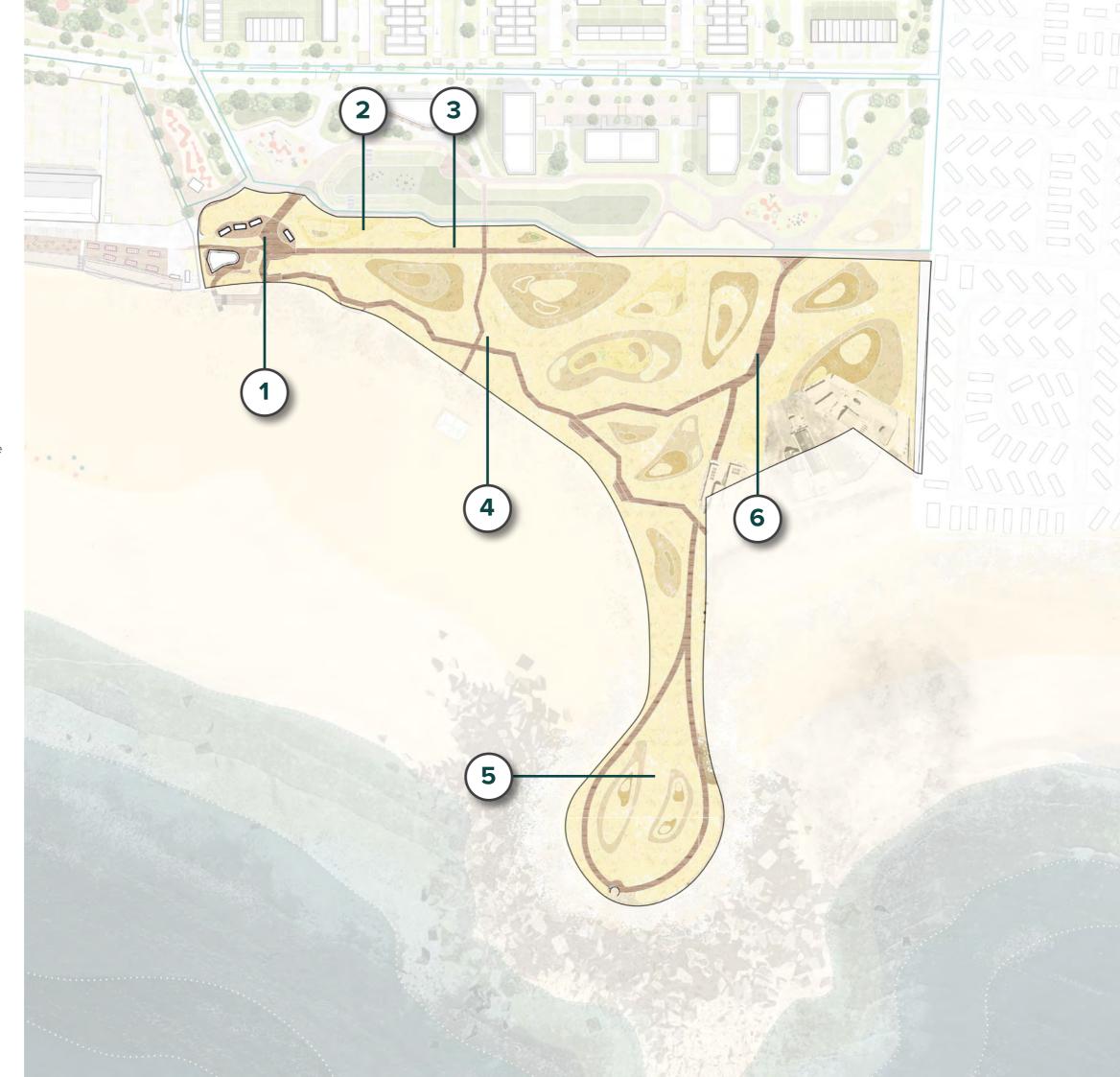
Boardwalk with viewpoints following Wales Coastal Path.



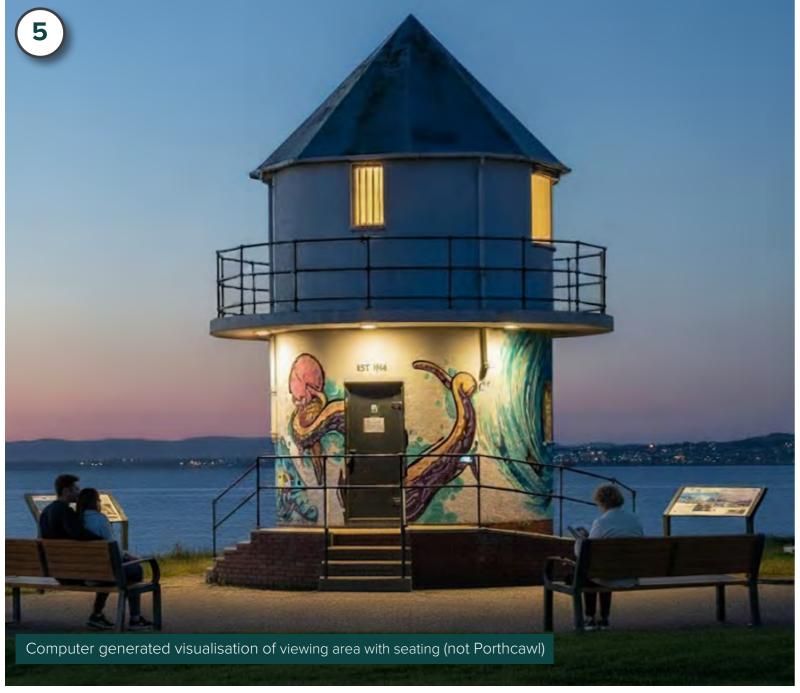
Enhanced watchtower viewpoint area with info-boards, seating and illuminated watchtower.

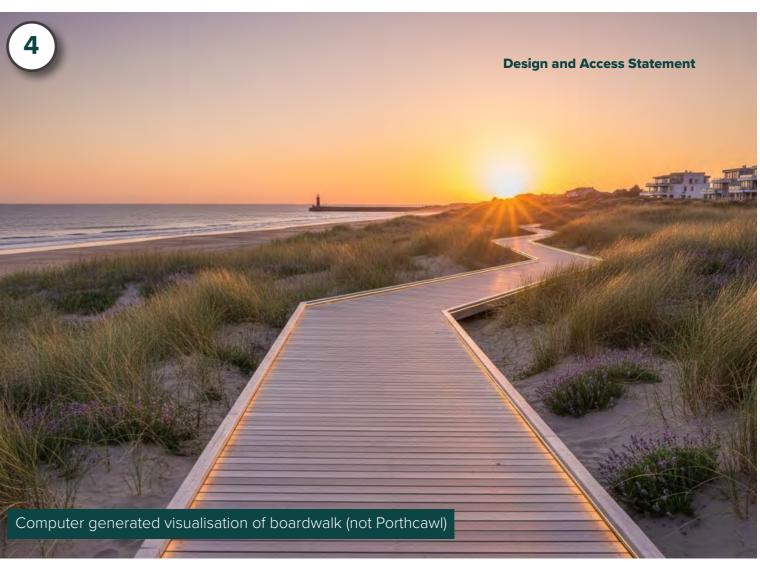


Rationalised existing road for lifeguard access.









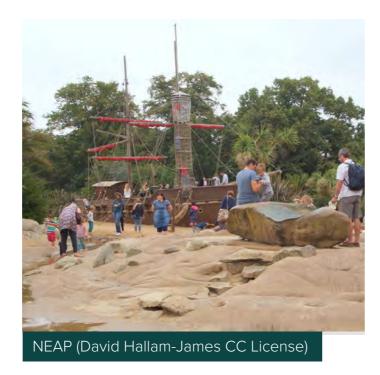


4.4 PLAY STRATEGY

A comprehensive, site-wide Play Strategy has been developed to provide a range of high-quality play opportunities for children and young people of all ages. The strategy aligns with Bridgend County Borough Council's policy and the latest Fields in Trust (2024) guidance, ensuring that the design, location, and accessibility of play spaces meet national and local policy requirements.

The approach promotes a playable landscape, in which formal, informal, and incidental play spaces are integrated throughout the development. Play has been a central consideration from the outset of the masterplan and forms a core component of the landscape and public realm strategy. The vision is to create a vibrant, family-friendly coastal neighbourhood where play and activity are embedded within everyday movement and social spaces.

Formal play spaces will be complemented by a network of natural and imaginative play opportunities, encouraging exploration, creativity, and active lifestyles. The design will create a connected network of play, ensuring safe, inclusive, and engaging environments for all age groups, with walking distances in line with Fields in Trust guidance.





Local Areas for Play (LAPs)

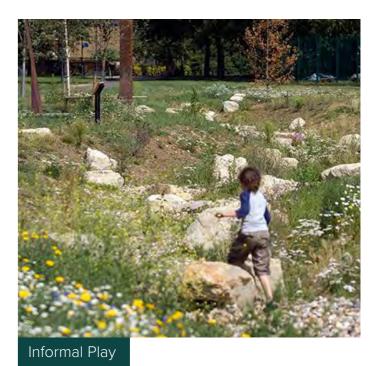
LAPs will provide the smallest scale of play opportunity, aimed at children aged 2–6 years. These spaces will typically comprise informal play features, such as low mounding, planting, and sculptural landscape elements, to encourage imaginative, exploratory play. Each LAP will include a soft landscape buffer of approximately 5 metres to ensure safety and separation from adjacent uses while maintaining visibility for supervision.

Local Equipped Areas for Play (LEAPs)

LEAPs will provide equipped play opportunities for younger children, typically aged 2–8 years. Each LEAP will include at least nine pieces of play equipment, six of which will be suited to children aged 4–8 years. Seating and shade will be provided to encourage family use, with low-level planting or fencing to create a sense of enclosure and safety while deterring dogs.

Neighbourhood Equipped Areas for Play (NEAPs)

A larger NEAP will be provided within a central, accessible location, offering a variety of play opportunities for older children and young people. The NEAP will include a broad range of equipment types and materials to support active, social, and inclusive play. It will be designed as a focal point for the wider community and integrated within a well-landscaped setting that promotes natural surveillance and accessibility.



Natural and Imaginative Play

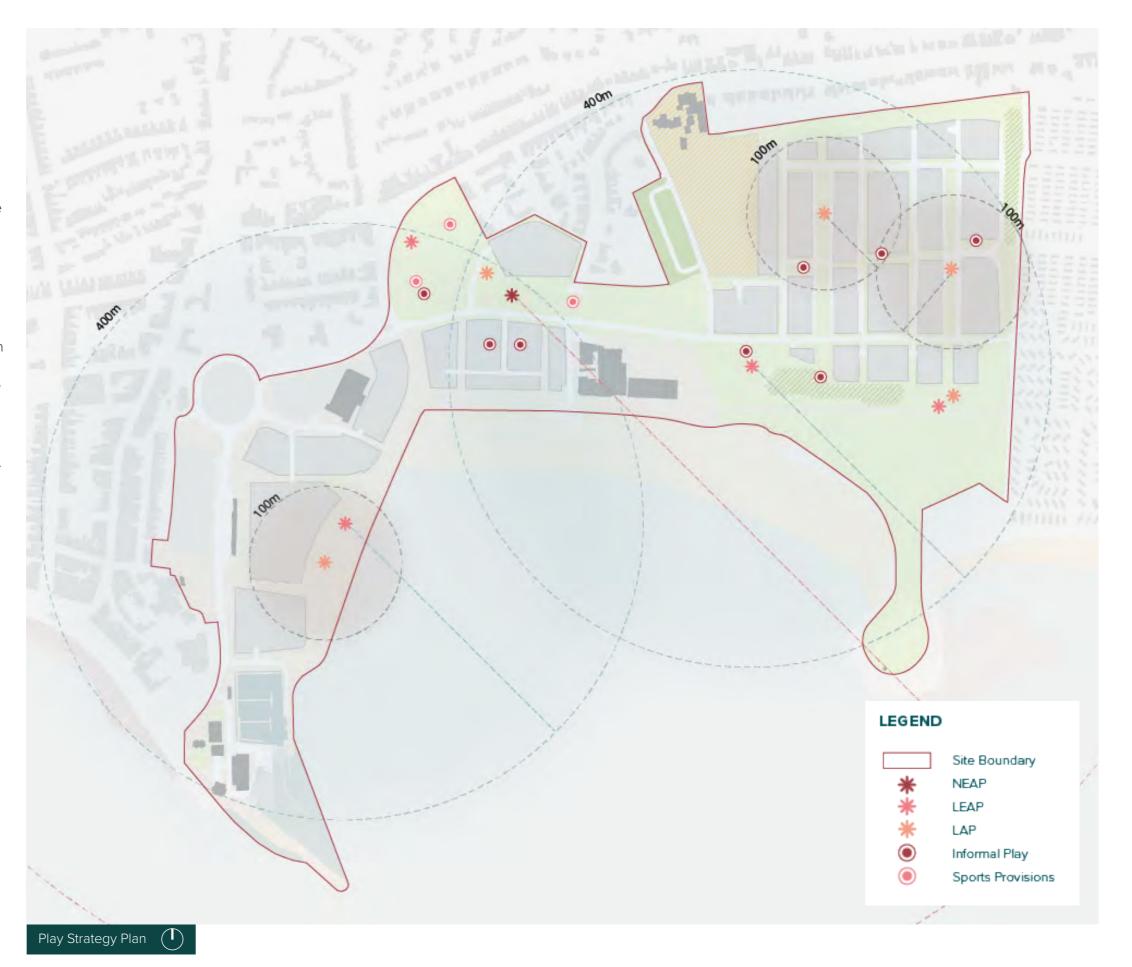
In addition to formal provision, natural and imaginative play will be embedded throughout the masterplan as part of the wider green infrastructure network. Playon-the-way features will be incorporated into key pedestrian routes and public spaces to encourage spontaneous play and exploration during everyday journeys.

Nature-based play will be particularly prominent within planted and woodland areas, including the former Pleasure Gardens (Monster Park) within Coney Beach, where woodland play opportunities will celebrate the site's historic landscape character. Natural play will also be integrated into SuDS corridors, green links, and open spaces, combining ecological function with creative, low-intervention play features such as timber structures, boulders, and landform sculpting.

Enhancement of Existing Facilities

The existing play areas within Griffin Park will be retained and enhanced to safeguard valued community assets and provide improved, inclusive, and accessible play environments. Enhancements will focus on upgrading play quality, surfacing, and equipment while strengthening connections to surrounding open spaces and the seafront promenade.

Together, these proposals will establish a diverse and inclusive play network that supports health, wellbeing, and social interaction. The Play Strategy forms an integral part of the Porthcawl masterplan's commitment to creating an active, family-friendly coastal environment, where play is interwoven with nature, movement, and placemaking.



4.5 GREEN INFRASTRUCTURE

Overarching Strategy

Large elements of the proposals is founded on a Green Infrastructure (GI) approach, ensuring that natural systems, biodiversity, and public open spaces are fully integrated throughout the masterplan. The GI strategy seeks to create a resilient, connected, and multifunctional landscape network that serves both people and nature; enhancing biodiversity while supporting well-being and climate resilience.

Across the site, the GI network will bring together existing coastal habitats, retained woodlands, new parks, green corridors, and tree-lined streets to form a cohesive and legible structure. It establishes a sequence of green and blue spaces that connect the dunes and waterfront with the inland neighbourhoods and Griffin Park, allowing wildlife and people to move freely through the landscape.

A rich mix of green spaces will be provided including:

- Formally designed parks;
- pocket greens; and,
- play spaces within naturalistic habitats and wetland features.

These areas will be designed to encourage recreation, play, food growing, and outdoor activity, supporting sustainable and healthy lifestyles for residents and visitors alike.









Responsive Green Space

The character of green spaces will respond to their setting:

- In the southern coastal areas, the landscape will draw inspiration from the maritime environment, integrating dune and grassland species suited to the exposed conditions.
- Moving north, the landscape will transition towards more sheltered, ornamental settings, evoking the character of traditional pleasure gardens with mixed woodland, flowering trees, and diverse planting palettes.

Blue infrastructure is a key component of the strategy. Sustainable Drainage Systems (SuDS) such as swales, rain gardens, and attenuation ponds will be incorporated throughout the public realm and green streets to manage surface water naturally, improve water quality, and add to the visual and ecological richness of the landscape.

The distribution of green throughout the scheme will ensure all new homes will be closely connected to green space, creating a natural and visually appealing environment across the masterplan.

The design will also deliver ecological enhancements including the integration of bird and bat boxes, nesting bricks, and hibernacula within new buildings and landscape features. Dark corridors and ecologically sensitive lighting will be incorporated to protect nocturnal species and ensure uninterrupted ecological connectivity.



4.6 ECOLOGY AND BIODIVERSITY

Ecology and Biodiversity Strategy

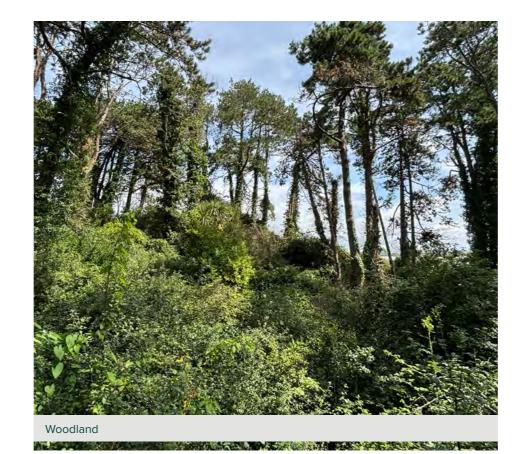
The proposals aim to create a living landscape framework that safeguards and enhances the site's ecological value while supporting sustainable and active lifestyles. The ecological approach follows a clear hierarchy:

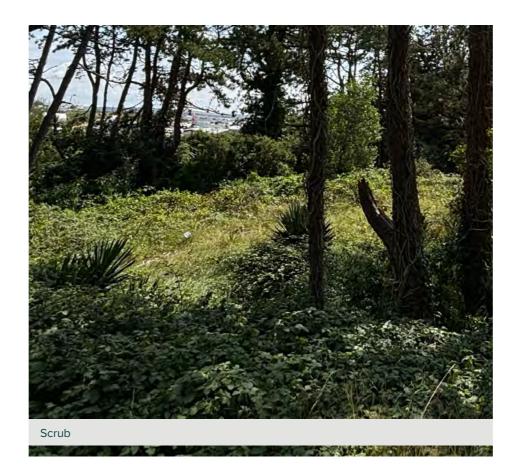
- First to avoid harm;
- Then mitigate unavoidable impacts; and finally,
- Enhance or replace habitats to deliver biodiversity net gain.

The most significant ecological measure is the retention and enhancement of the mobile sand dunes, identified as the site's highest-value habitat. Where interventions are unavoidable, such as the partial infilling of the Sandy Bay Bowl, dune material will be carefully translocated to adjacent retained dune areas to preserve soil structure and seed stock. The scheme also avoids encroachment on Rhych Point, protecting its species-rich maritime grassland and rocky shoreline SINC, which are of regional ecological importance.

Although a new link road is required through the southern section of Monster Park, its design limits ecological disturbance by retaining the northern section to maintain habitats for the local reptile population. An underpass will be incorporated where the new road intersects a green corridor, ensuring safe wildlife movement between Griffin Park, Monster Park, and the dune system.

To further enhance biodiversity, species-rich maritime grassland affected by the works will be stockpiled and reused across suitable retained areas, enabling the creation of new grassland communities that reflect the local sandy soils and maritime conditions.









The development also embeds green infrastructure as a central structuring element of the landscape design. A continuous ecological corridor runs from Griffin Park through Monster Park and into the dunes, creating a multifunctional landscape that supports wildlife, recreation, drainage, and amenity. This corridor will be complemented by a network of interconnected open spaces, streets, and gardens designed to provide access to nature for all.

New planting will prioritise a rich diversity of native species, supplemented by resilient non-natives suited to the maritime climate. Species will be selected to provide nectar, seeds, and berries throughout the year, supporting pollinators and birds, and contributing to climate resilience.

Ecologically sensitive lighting will be introduced along the boundaries of potentially ecologically rich areas and new green spaces to reduce disturbance to nocturnal species, particularly bats.

Recognising that some biodiversity net gain (BNG) may be challenging to deliver entirely within the site, opportunities for off-site habitat creation will be explored in accordance with the BCBC Biodiversity Duty Plan 2022–2025, ensuring measurable and lasting ecological benefits.

All proposals will be supported by up-to-date ecological surveys, guiding habitat enhancement, species protection, and ongoing monitoring through the detailed design and delivery stages.



TREES

Design Approach

Tree planting is a key component of the landscape proposals, contributing to biodiversity, micro climate regulation, and seasonal character throughout the development. The strategy combines the retention of existing woodland with carefully selected new species to create a resilient and distinctive landscape structure.

The approach draws on the principles set out in the Welsh Placemaking Charter prioritising identity, ecological value, and long-term resilience. Trees are used not only to define character areas but also to reinforce the sense of continuity across the site, from the exposed coastal edge to the more sheltered inland neighbourhoods.

Existing woodland belts around Monster Park and Griffin Park are integral to the local landscape character and have sought to be retained and reinforced wherever possible within the proposals, maintaining valuable habitats and supporting wildlife. Where new trees are introduced adjacent to these areas, species selection responds to the existing woodland composition to ensure visual and ecological coherence.

Tree Planting

New tree planting will provide a variety of functions and experiences across the masterplan:

- Street Trees: Form green corridors and tree-lined avenues, supporting the active travel network while improving air quality and comfort through shade and enclosure.
- Amenity Trees: Offer seasonal colour, biodiversity, and shade within public open spaces and residential courtyards.
- Specimen Trees: Create moments of visual interest and orientation, marking key nodes, entrances, and focal points.
- Woodland and Parkland Trees: Extend existing habitats, reinforce ecological networks, and frame open green spaces with informal planting arrangements.

To ensure healthy long term growth, all trees in paved areas will be planted in strata cell systems that promote root development and protect pavement integrity. A comprehensive maintenance and management plan will be developed to support establishment and mitigate long-term maintenance costs.

Species selection has been carefully zoned to respond to local micro climates and landscape character, including the maritime conditions along the waterfront. The palette combines native and non-native species to ensure ecological diversity and climate resilience, drawing guidance from the Trees and Design Action Group's Tree Species Selection for Green Infrastructure.



Pinus sylvestris | Scots Pine (PlantPARTNER CC License)



Ulmus 'Columnella' | Elm 'Columnella' (PlantPARTNER CC License)





Sorbus aria 'Lutescens' | Whitebean 'Lutescens' (PlantPARTNER CC License)



Acer campestre | Field Maple (PlantPARTNER CC License)

Proposed Tree Species by Character Area

Salt Lake:

- Waterfront: Scots Pine (Pinus sylvestris), Thornless False Acacia (Robinia pseudoacacia 'Bessoniana')
- Inland: Columnar Elm (Ulmus 'Columnella'), Leopold's Sycamore (Acer pseudoplatanus 'Leopoldii'), Bastard Service Tree (Sorbus hybrida 'Gibbsii')

Coney Beach Boardwalk:

- Waterfront: Scots Pine (Pinus sylvestris)
- Residential: Service Tree (Sorbus × thuringiaca 'Fastigiata'), Snow Gum (Eucalyptus pauciflora niphophila)

Coney Beach Park:

 Midland Hawthorn (Crataegus laevigata), Turkey Oak (Quercus cerris), Crab Apple (Malus sylvestris), Holm Oak (Quercus ilex), Snowy Mespilus (Amelanchier lamarckii), Field Maple (Acer campestre)

Sandy Bay Gardens:

 Jounama Snow Gum (Eucalyptus pauciflora debeuzevillei), Holm Oak (Quercus ilex), Scots Pine (Pinus sylvestris), Swedish Whitebeam (Sorbus aria 'Lutescens')

Sandy Bay Village:

• Ornamental Pear (Pyrus calleryana 'Chanticleer'), Columnar Elm (Ulmus 'Columnella')

East-West Access Road:

 Dunkeld Larch (Larix × eurolepis), Swedish Whitebeam (Sorbus aria 'Lutescens')



4.8 PLANTING

The planting strategy prioritizes supporting nature and wildlife while ensuring the planting also delivers essential multi-functional benefits, including micro-climate control, food production, water management, visual screening, and aesthetic appeal.

Principles

The proposed planting is guided by the following principles to ensure a biodiverse, resilient, and integrated landscape:

Everyday Connection: Planting will be integrated throughout the new development to allow residents and visitors an everyday connection with nature.

Biodiversity & Habitat Driven: The core of the strategy is driven by enhancing biodiversity and habitat creation, focusing on the ecological value of every planted area.

Contextual Palettes: Planting palettes will strictly reflect the local species, typologies, soil conditions, microclimate, and coastal location of the site.

Resilient Mix: We will utilize a mix of native and nonnative species, prioritizing nectar-rich varieties to deliver a highly biodiverse and resilient landscape.

Low Maintenance: Robust planting species will be selected to ensure low long-term maintenance requirements.

Long-Term Management: A comprehensive long-term management and maintenance plan will be drawn up, defining the precise maintenance regime and requiring agreement from both the Applicant and BCBC (Bridgend County Borough Council).

Year-Round Interest: Planting will be designed to celebrate the seasons and provide continuous visual interest throughout the year.

Typologies

The following planting typologies will be utilized to distinguish the different character areas across the site, reflecting the unique coastal and urban settings of Porthcawl:

Maritime: Robust coastal areas requiring species to thrive in salt-laced, windy conditions. Species include: Sea kale, Sea holly, Remote sedge.

Dune Edge: Transitional planting between natural dune grasses and more ordered areas (e.g., Sandy Bay). The mix will be rich in native sedges and flowering species. Species include: Native sedges, Dove's foot cranesbill, Sea thrift.

Coastal Grasses: Focused on the enhancement of existing dunescape areas to improve overall habitat value. This will involve reinforcing and enriching existing native grasses.

Woodland / Woodland Edge: Targeted at existing woodland pockets toward the northern boundary. This typology will favour native shrubs, ferns, and perennials. Shrubs might include: Dogwood, Sambucus, Hawthorn, Dog rose. Underplanting will feature native ferns and sedges, with potential for some areas sown with a native hedgerow edge seed mix.

Urban Ornamental: Designed for urban interiors, these palettes will emphasize drought tolerance, nectar richness, colour, and scent. Species might include: Salvia, Agapanthus, Achillea, §, and ornamental grasses such as Miscanthus and Festuca.

SuDS Mix: Species specifically chosen for their capability to tolerate both wet and dry conditions (essential for Sustainable Drainage Systems). This robust mix includes: Native ferns, Bugle, Angelica, Sedge, Common Knapweed, Dogwood, Wild teasel, Black medick, Daffodils, Red switch grass, Great burnet, Devil's bit scabious.



Sea Kale | Crambe maritima (Bob Jones CC License)



Sea Holly | Eryngium maritimum (Bernard Dupont CC License)



Remote Sadge | Carex remota (E Wusk CC License)



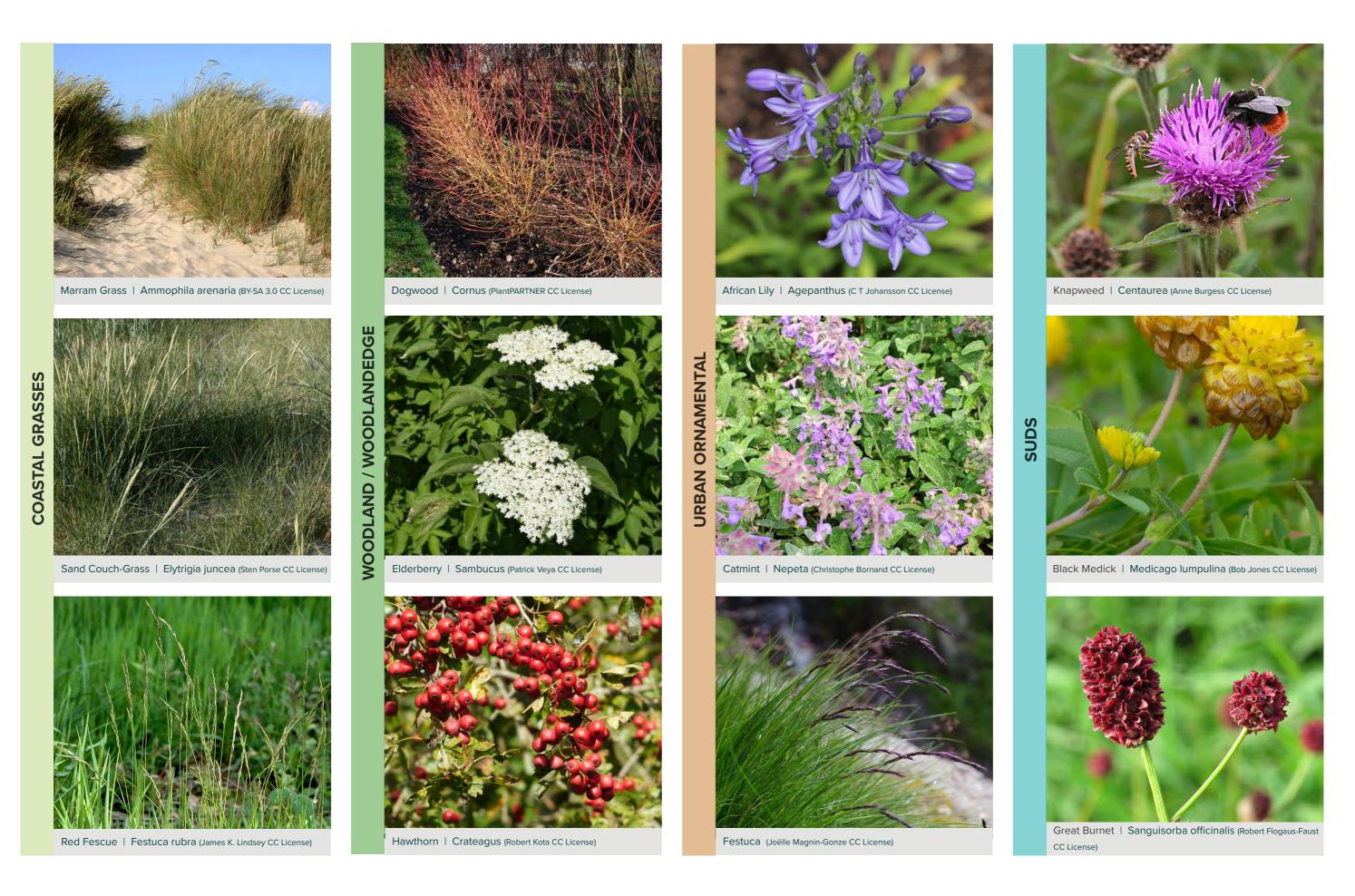
Pendulous sedge | Carex pendula (Len Worthington CC License)



Dove's-foot Crane's-bill | Geranium molle (Peter Friedman CC License)



Sea Thrift | Armeria martima (Jarro Nevbaru CC License)



4.9 MATERIALS

4.10.1 Materials Principles

Materials will be used in a consistent way to avoid a patchwork appearance and to contribute to a strong visual identity to the development. The material strategy prioritizes durability, context, and accessibility to deliver a high-quality public realm.

Quality & Durability

Hard-Wearing: Materials will be selected to withstand projected footfall and vehicle loads.

Coastal Resilience: Robust, resilient materials proven to endure the coastal climate (salt, wind) to ensure longevity and low maintenance.

Character & Context

Local Tie-in: The palette reflects the local character and context of Porthcawl.

Conservation Area: A high-quality material palette (e.g., natural stone) will be used within the Conservation Area to reflect local history and heritage.

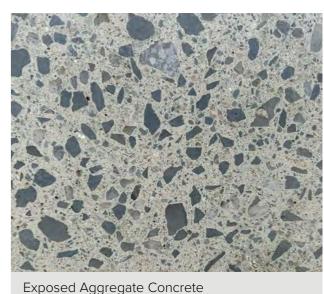
Accessibility

Safe Surfaces: Materials will ensure surfaces are smooth, even, and slip-resistant, with clean levels and joints.

Visual Contrast: High contrast will be provided where necessary for safety and accessibility standards.















Clay Setts

Permeable Paving







Timber Cladding

Boardwalk

4.10 STREET FURNITURE & WAYFINDING

Street Furniture and Lighting Strategy Summary

The strategy for street furniture and lighting aims to achieve a coherent aesthetic across the development while promoting high standards of accessibility and environmental protection.

Consistency and Character

All street furniture and lighting adheres to a common palette of materials, specifically concrete, wood, and powder-coated metal, to maintain a cohesive identity across the masterplan. However, gentle style variations are also utilised to reflect the identity of specific character areas. For example, furniture within and near to the conservation area and listed buildings responds to historical character with more traditional natural elements, and the harbourside utilises more industrial materials and finishes that reflect the areas dock and rail history.

Furniture Requirements and Resilience

Furniture will be robust to withstand the coastal setting and extreme weather. Accessibility is paramount, requiring various seating types (with/without backrests and armrests) and spaces for wheelchair users. To ensure comfort and rest along key routes, seating is provided a minimum of every 50 metres. Furthermore, the selection process encourages sustainability by assessing the embodied carbon of products and their potential for long-term repair and upgrading.

Responsible Lighting Design

Lighting design is focused on minimising street clutter by requiring light fittings to be concealed within the landscape wherever possible. Columns are proposed to be elegant and integrate multiple functions (lighting, signage, CCTV) to reduce visual impact.

Crucially, all artificial lighting is be designed to mitigate light pollution spill and glare on existing and future homes, more detail on this element can be found within the lighting Technical Design Note.





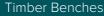


Signage and Story Telling



Lighting Columns







101

4.11 BUILT FORM

Density

The proposed density is intended to reflect the existing and proposed site uses to ensure that the development maximises on the available land use opportunities with appropriate housing typologies. The density framework should be read in conjunction with the built form framework.

Generally, the highest densities will be located around the waterfront. This is will be the most appropriate approach given that taller built form will be required to create a sense of enclosure and presence to an otherwise expansive area and this will generally be reflected by apartments. In addition, the western areas around Salt Lake and Coney Beach will reflect the highest densities due to the proximity to the town centre maximising on the commercial offering and transport facilities afforded by the town centre in an urbanised environment.

Densities around the eastern areas in Sandy Bay will generally taper off to reflect a more suburban and residential character, characterised by family housing and less publicly intensive uses. These typologies will be generally reflective of the surrounding built form around New Road and a farther location.

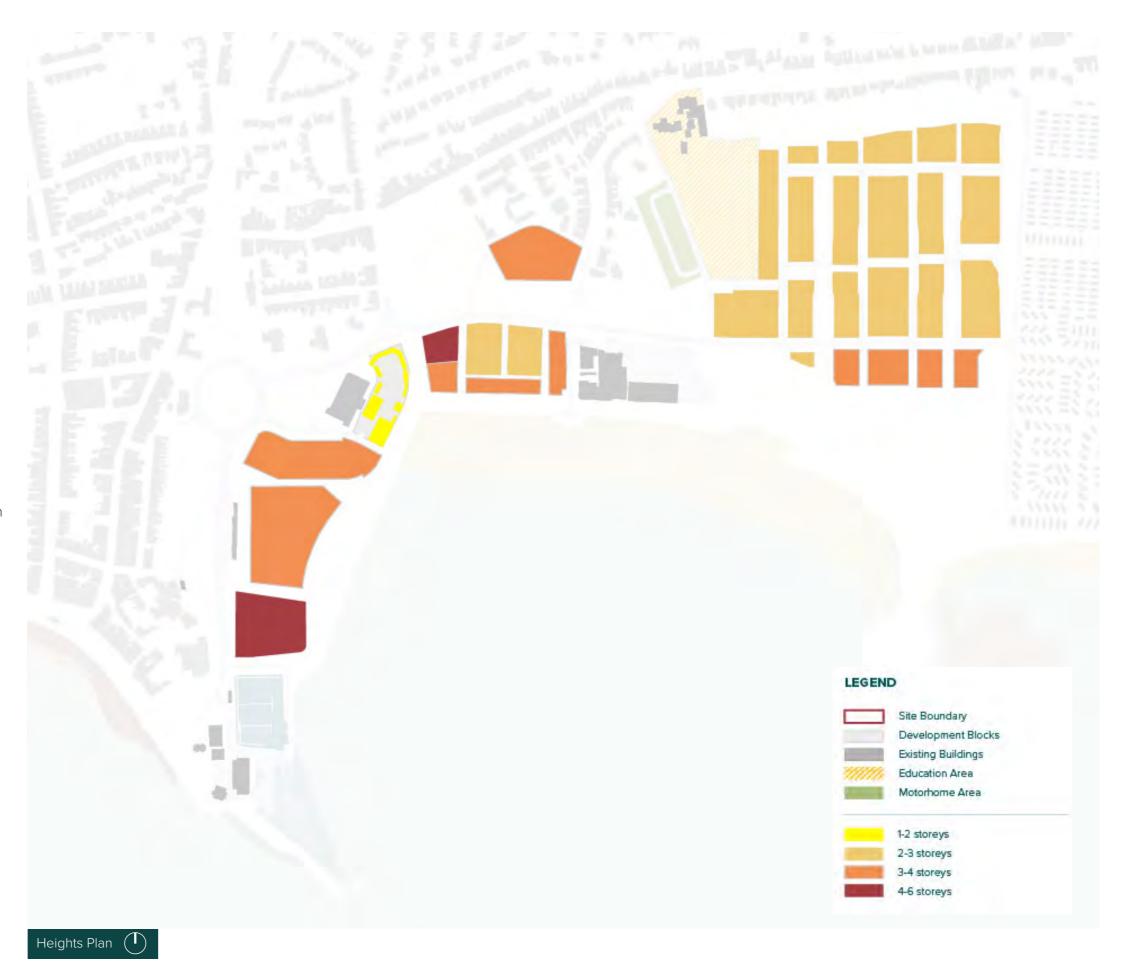


Heights

The proposed scale is intended to reflect the surrounding existing built form, which is generally between 2-3 storeys. A slight increase in scale will be strategically proposed in areas reflecting most sustainable land uses and areas that will positively contribute to the wider townscape by creating landmark features.

Some of these areas will include Salt Lake, due to proximity to the town centre and public transport provisions creating an urban townscape. Limited locations within Salt Lake and Coney Beach areas are proposed to be up to 6 storeys, creating a visual landmarks at prominent locations and gateways aiding legibility.

Lastly, wider open spaces and the waterfront will generally be lined by buildings up to 4 storeys to form a significant presence along an otherwise extensive open space.



4.12 LAND USE FRAMEWORK

Housing

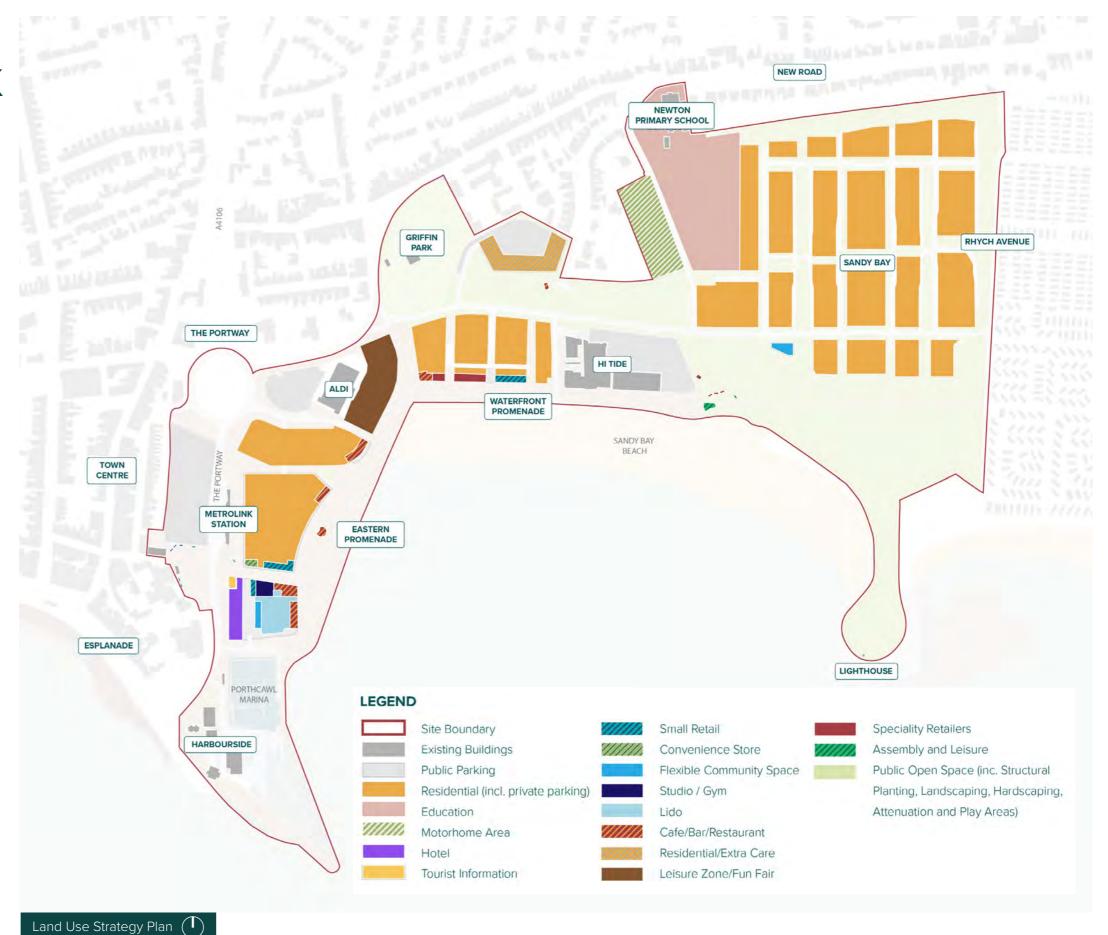
The development will provide up to 980 homes, spread across the site. Sandy Bay will comprise predominantly of residential uses with supporting non-residential uses including public open space with doorstep play and a primary school. The area is intended to achieve an informal and more intimate character that caters to the limited number of residents.

Salt Lake and Coney Beach, alternatively, will feature a mixed-use arrangement that incorporates residential uses with various other uses, including public open spaces that are highly focused around the general public use. The proposed non-residential uses will be primarily focused towards the interface with the waterfront and along key desire lines, complimenting and benefiting from the positive outlooks and passive footfall.

The commercial/food and drink uses will be primarily focused around Salt Lake Central onto the Eastern Promenade/Dock Street link and Coney Beach onto the Waterfront Promenade, to enhance the beachfront commercial offering. In addition, a hotel, food and drink, gym and public pool uses are proposed to be created in Salt Lake South creating a new leisure destination located in close proximity to the Bus Metro and offering trip generation benefits to the town centre.

Salt Lake North, adjacent to the existing Aldi Store comprises of a mix of new homes and a leisure zone capable of housing a small scale fair ground and other complimentary uses such as a performance space and retail uses.

In terms of the less public- facing uses, the Coney Beach North parcel will feature an extra care facility alongside a public car park. These facilities will benefit from the adjacent public open space and range of nearby local amenities. In addition, The areas alongside Newton Primary School will feature a new motorhome area replacing the existing provisions and land for future educational use.



Meanwhile Uses

Meanwhile uses forms part of the overall vision for the waterfront regeneration, providing an opportunity to activate the site early, test and evolve initiatives, and establish a sense of place ahead of longer-term development. The design principles guiding the meanwhile phase ensures that temporary interventions are purposeful, inclusive, and contribute positively to the identity and evolution of the waterfront area in a manner that delivers the overarching vision.

Early Activation and Placemaking

Meanwhile uses are designed to bring life to the site from the outset, creating reasons for people to visit and engage with the waterfront before permanent development takes shape. Early activation through events, pop-up uses, and adaptable structures will help shift perceptions of the area, building awareness and local ownership over time.

Two key areas are proposed to come forward early in the overarching delivery of the site include Hillsboro South Pocket Park with pop up spaces and the leisure zone to the east of Aldi capable of housing a fun fair and complimentary uses.

This approach embraces flexibility, allowing for these spaces and associated structures to evolve, relocate, or scale according to demand. This adaptability ensures that investment in temporary infrastructure, such as modular units, lightweight landscape interventions, and event spaces can be retained, reused, or integrated into the permanent masterplan as it develops.

These areas and meanwhile proposals will support local enterprise and community activity, creating accessible opportunities for local entrepreneurs, creatives, and community groups. Spaces for small-scale food and beverage uses will help nurture local talent and establish a distinct identity rooted in community participation while creating interesting destinations within the waterfront.

Salt Lake Promenade will be designed in the long term to enable opportunities for a meanwhile functionality throughout the year. The space will have the flexibility to support and enhance the recreational uses across the site area by being able to accommodate seasonal events, pop-up food and weekly markets.

Testing and Learning

The meanwhile proposals provide a live testing ground for the long-term masterplan. Insights gained from these interventions have the potential to go onto inform future detailed designs and phasing, ensuring that future development responds to observed patterns of use and public feedback.

While temporary by nature, the meanwhile uses are conceived as the first chapter of the regeneration story. Their placement, design language, and operational model have been aligned with the long-term regeneration vision to ensure that early investment contributes to the enduring quality, accessibility, and identity of Porthcawl's Waterfront.



East Quay - Meanwhile Use (Credit: RIBA Journal, Jim Stephenson)



Blue House Yard - Meanwhile Use (Source: Meanwhilespace.com)

4.13 CHARACTER FRAMEWORK

Character Generators

The Welsh Placemaking Charter highlights the importance of identity as a core principle in shaping new development. This principle encourages proposals to engage with the distinctive qualities of place, from local heritage and language to patterns of the built and natural environment, creating a response that feels authentic and rooted.

Building on this, the proposed character area framework takes the existing qualities of the site and surrounding context as a foundation, illustrating how future development can evolve in dialogue with its setting. Each character area is conceived to have a distinct architectural identity, shaped by its relationship to context, whether that be the landscape, existing structures, or local patterns of use. The aim is to create places that are recognisable, memorable, and clearly of their setting within Porthcawl.

A series of character generators, both existing and proposed, have informed the development of the framework. These guide how each area might vary in massing, density, appearance, boundary treatment, and public realm, ensuring a cohesive yet varied urban form.

The accompanying plan identifies the constraints, opportunities, and land use patterns that inform this approach, setting out how built form can best respond to and amplify the qualities that make this place unique.



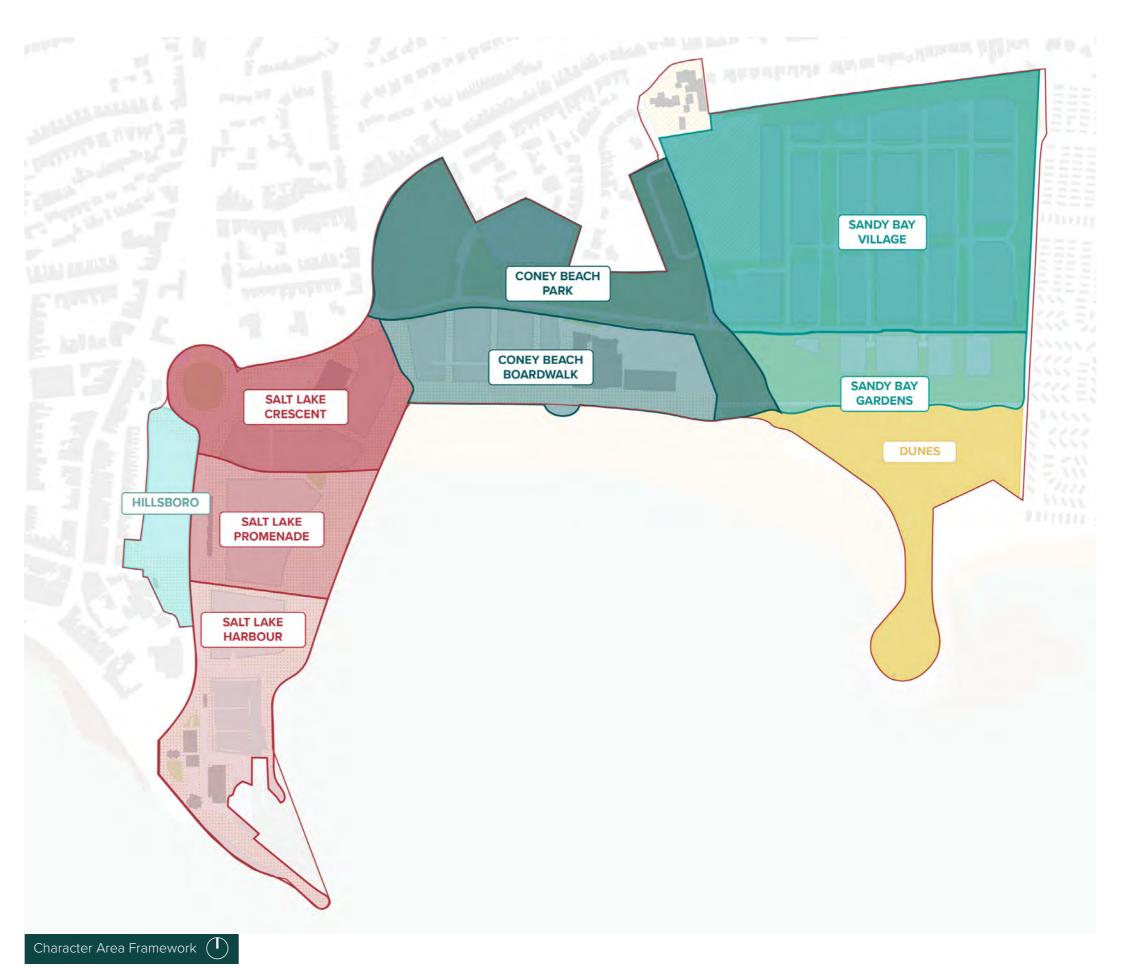
Character Area Framework

Building on the character generators, a network of distinct character areas are illustrated to help differentiate between areas of the site. This is illustrated within the character areas framework plan.

These are intended to inform proposals and ensure they respond to the specific requirements and challenges of the different parts of the site while creating unique locations.

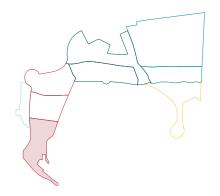
The three primary character areas (Salt Lake, Coney Beach, and Sandy Bay) will be incorporated as part of delivering harmonious variety and distinctive neighbourhoods. There are key characteristics set out for each Character Area.

The following section provides a breakdown of the individual character areas.



4.14 SALT LAKE HARBOUR

Character Area Overview



Character Generators

- Proximity to town centre and public transport;
- Waterfront;
- Marina and heritage buildings;
- Portway;
- Active travel links; and,
- Views to the bay.

Density and Heights

Due to the proximity to the town centre and the waterfront, appropriate prominence and density is proposed.

Built Form and Massing

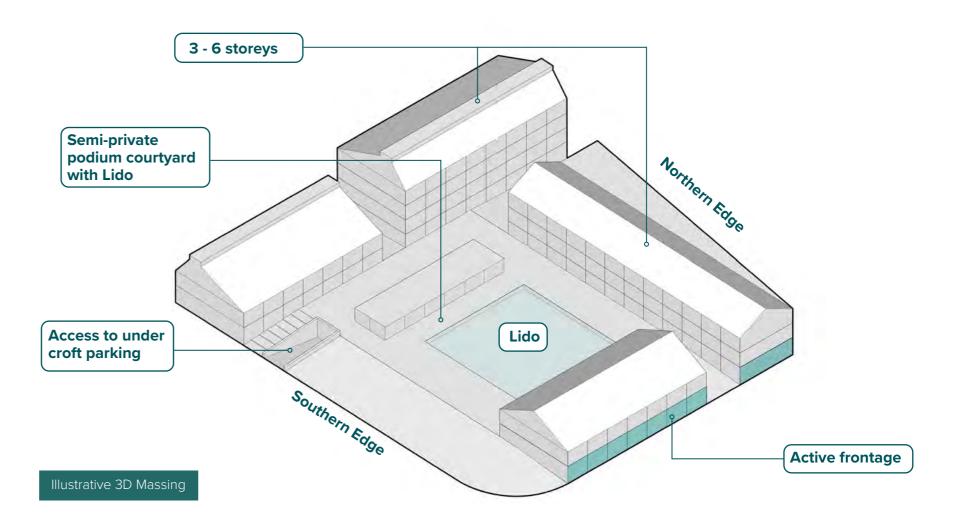
Massing focusses the taller built form towards the northwestern corner with the remainder of the built form tapering down, in order to provide screening from Portway and views to the waterfront. Buildings aim to be simple and elegant with pitched roofs, drawing influence from the harbour buildings to the south.

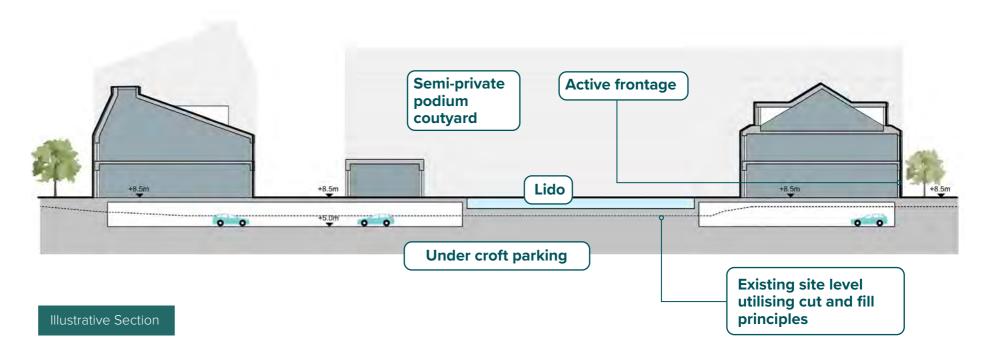
Block Typology

A courtyard block surrounding a lido is proposed, with under croft parking below. Significant active frontage animating the ground floor areas is anticipated, particularly facing the waterfront, the active travel link along the northern edge and harbour area along the southern edge.

Dwelling and Unit Typology

Typologies predominantly consist of apartments and ground floor commercial/community units along with facilities to support the Lido.







Simple and elegant massing with roof forms reflecting the existing marina buildings

Contemporary
materiality and detailing
drawing on existing
marina character

Active frontage on ground floor



Boundaries

A similar boundary treatment to the existing harbour area buildings is anticipated, creating an urban character with maritime influences. This is reflected through the use of sheer, hardscaped boundaries to the public realm, where commercial/community uses are proposed. Any residential uses achieve a similar character through shallow defensible spaces.

Private Amenity

Private amenity is reflective of the context and the appropriate dwelling/ unit typologies. This is focused around private balconies to maximise on the available waterfront views and private/semi-private Lido above undercroft parking. The latter is clearly delineated from the public space.

Detailing and Materials

The detailing and material elements draw on the marina character with contemporary interpretations encouraged. Ground floor elevations comprise of rubble stone with masonry plinths as a point of continuity with more flexibility on materiality to the upper floors. Proposals introduce predominantly muted grey tones.

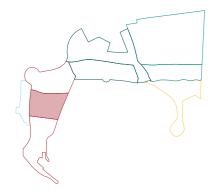


Wapping Wharf, Bristol (Source: MEP Engineering Solutions)

Precedent images illustrating new developments with historic waterfront character

4.15 SALT LAKE PROMENADE

Character Area Overview



Character Generators

- Proximity to town centre and public transport;
- Waterfront;
- Portway;
- · Active travel links; and,
- Views to the bay.

Density and Heights

Due to the proximity to the town centre and the waterfront, appropriate prominence and density is proposed.

Built Form and Massing

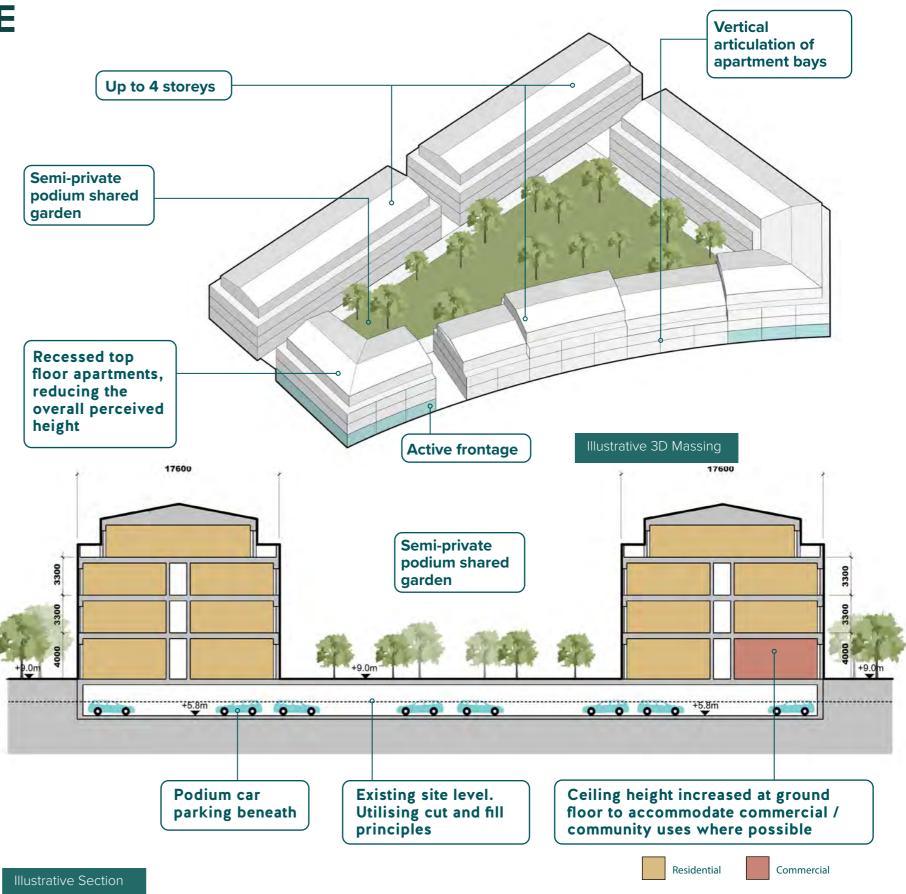
A sweeping curved terrace is proposed as a key feature of Salt Lake Promenade, creating a strong architectural gesture whilst simultaneously embracing a key landscape space in front. It is anticipated that the facade is articulated with a vertical rhythm of bays, defined by rainwater goods or subtle recesses. Gentle changes in bay width are encouraged to create some variety. To minimise the overall volume, a minor break in the curved terrace is introduced along with subtle height changes. Recessed upper floors, set back from the primary façade line to reduce the overall perceived height. Projecting canopies at ground floor are suggested, providing shade and shelter for users. Porches are integrated into the ground floor assembly, providing small, covered spaces for resident access into the dwellings above.

Block Typology

A courtyard block is proposed with undercroft parking and active frontage animating the ground floor areas where possible.

Dwelling and Unit Typology

Typologies predominantly consist of apartments and ground floor commercial/community units.









Boundaries

Where commercial/community uses are proposed, boundary conditions consist of hard landscape, with invitations to sit, dwell and engage with public activities within. Shallow defensible spaces with opportunities for soft landscape are implemented for ground floor residential uses.

Private Amenity

Private amenity is reflective of the context and the appropriate dwelling/ unit typologies. This is focused around private balconies to maximise on the available waterfront views as well as private/semi-private courtyards. The latter is clearly delineated from the public space to ensure stewardship by residents.

Detailing and Materials

Timber cladding is proposed to define the ground floor elevation with a low-level masonry plinth. This is articulated with timber curtain walling and projecting timber canopies for the areas with active ground floor functions. Recessed timber porches are integrated within the ground floor zone, providing access for the residents living above. For levels 02 and 03 the materiality changes to a combination of either stone, brick or subtly pastelcoloured render. Material change thresholds are concealed with rainwater goods or recesses within the façade.



Boundaries - Shallow defensible space with clear public/private demarcation (Credit: Feilden Clegg Bradley Studios)





Accordia, Cambridge (Credit:

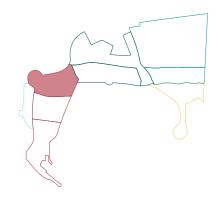
Feilden Clegg Bradley Studios)

Precedents illustrating new town centre residential developments



4.16 SALT LAKE CRESCENT

Character Area Overview



Character Generators

- Proximity to town centre and public transport
- Proximity to Griffin Park
- Waterfront
- Portway
- Active travel links
- Views to the bay

Density and Heights

Due to the proximity to the town centre and the waterfront, appropriate prominence and density is proposed.

Built Form and Massing

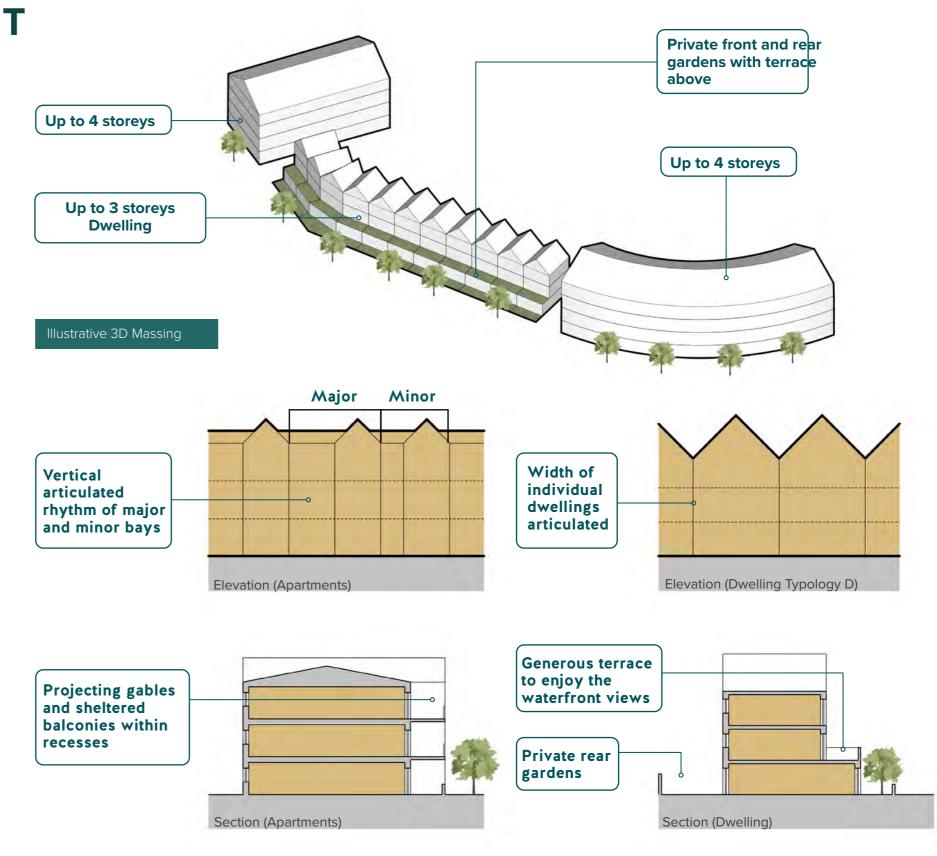
An elegant, curved terrace is proposed which wraps around the existing supermarket. Larger urban blocks 'bookend' the terrace and emphasize the sense of arrival from the town centre and make a purposeful connection to Griffin Park to the north. Buildings adopt a vertical grain with a combination of projecting gables and recessed facades, drawing inspiration from many of the Victorian streets within Porthcawl town centre. Projecting balconies within the recessed areas provide a greater degree of privacy and shelter. For the larger urban blocks (apartments), the horizontal rhythm consists of a major bay and a minor bay interspersed to create a subtle variety within the elevation length. For the individual dwellings, the width is articulated by either recesses within the façade, changes in material, or expressed rainwater goods.

Block Typology

Terrace and apartment blocks are proposed with parking to the rear.

Dwelling and Unit Typology

Typologies consist of predominantly individual homes and apartments.



Illustrative Section and Elevation

Residential



Changes in material / colour creates variety and interest

Projecting gables and sheltered balconies within recesses

Small front gardens create defensible space





Egham West (Credit: Timothy Soar)

Boundaries

To the front, small front gardens create a defensible threshold between the private dwelling and the public realm. To the rear of individual dwellings, a 1.5m masonry wall provides privacy for back gardens, with on street parking beyond.

Private Amenity

Private amenity space is incorporated through small front gardens, larger back gardens (for individual dwellings) and upper terraces and balconies where possible.

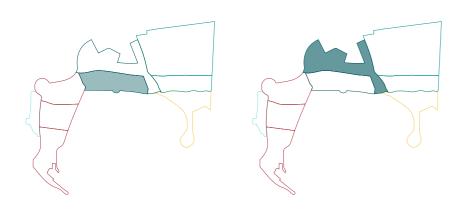
Detailing and Materials

The recessed areas of the façade consist of stone or dark brick, creating a degree of uniformity across the terrace elevation. The projecting gables provide an opportunity for individualism, the materiality changes to a pastel-coloured render or slate. Grey standing seam metal forms the mansard roofs above, creating material continuity with other character areas.

Precedents illustrating new town centre residential developments

4.17 CONEY BEACH BOARDWALK AND PARK

Character Area Overview



Character Generators

- Linear Park
- Waterfront
- Active travel links
- Views to the bay

Density and Heights

Due to the proximity to the town centre and the waterfront, appropriate prominence and density is proposed.

Built Form and Massing

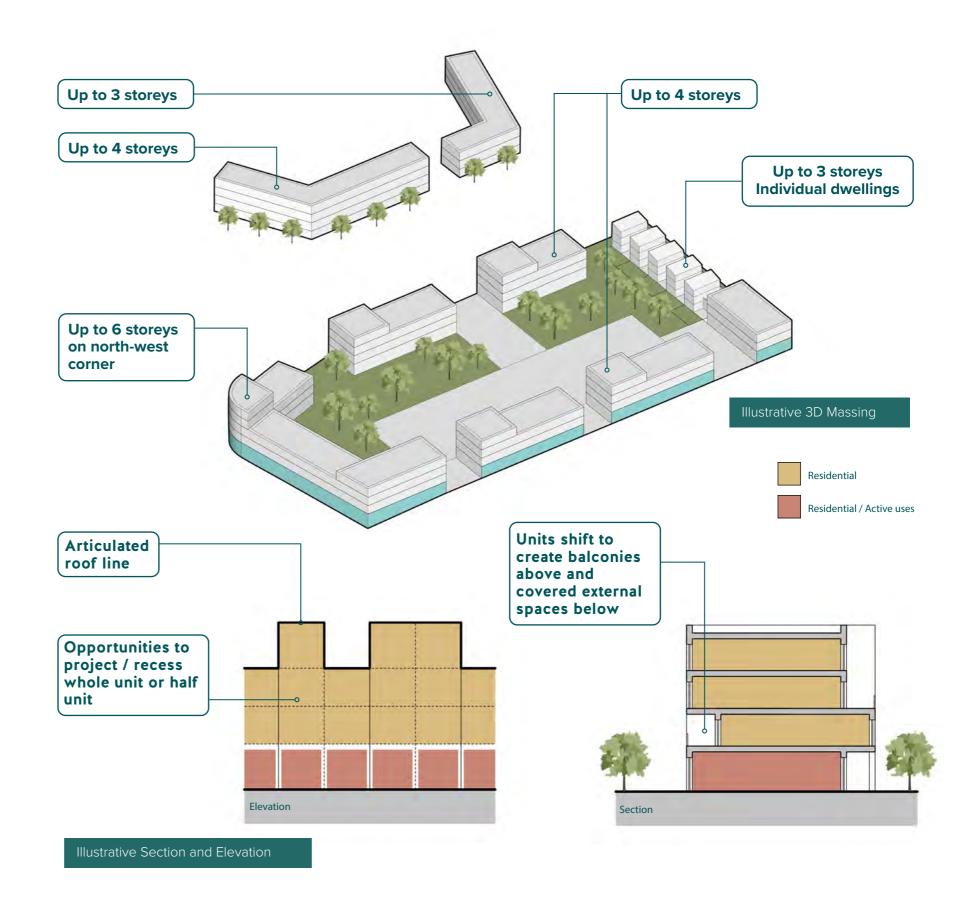
The façade adopts a more playful composition in this area, consisting of recessed and projecting bays of different heights and widths. Protruding sections provide shelter and a covered threshold for ground floor active uses below. Punched windows are carefully arranged to avoid repetition and uniformity. Large windows offer key views to the sea from living spaces within the dwellings. Protruding and Juliet balconies add a finer grain of detail. Roofs are typically flat and stepped along the building length to create an articulated roof line. A taller piece is proposed to the north-west corner creating a strong sense of arrival and orientation marker. The is also an ambition to introduce a subtle curve to the corner.

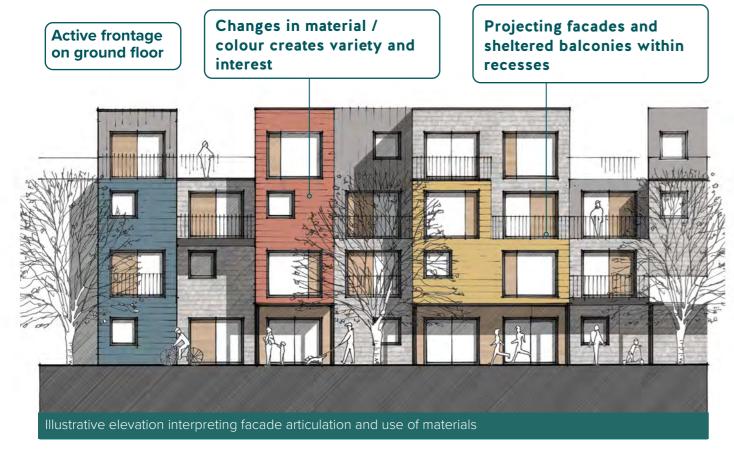
Block Typology

A series of courtyard blocks are proposed with parking to the rear.

Dwelling and Unit Typology

Typologies predominantly consist of apartments and ground floor commercial/community units.





Boundaries

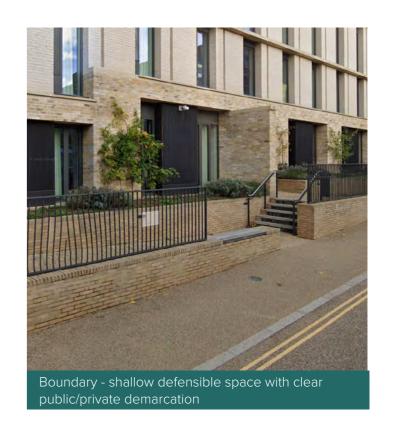
Where commercial uses are proposed boundaries consist of predominantly hard landscape, with invitations to sit, dwell and engage with public activities within. Shallow defensible spaces are implemented for ground floor residential uses.

Private Amenity

Private amenity is reflective of the context and the appropriate dwelling/ unit typologies. This is focused around private balconies to maximise on the available waterfront views and private/semi-private courtyards providing shared amenity space. The latter is clearly delineated from the public space to ensure stewardship by residents.

Detailing and Materials

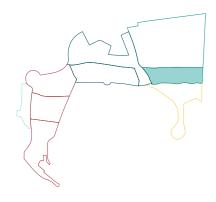
There is an ambition to use slate for the façade, complemented by grey metal standing seam cladding. The projecting elements can be animated with punchy colours formed from render or anodized metal. Timber is introduced into smaller areas, such as the doors to the balconies and surrounds to active uses on the ground floor.





4.18 SANDY BAY GARDENS

Character Area Overview



Character Generators

- Waterfront
- Active travel links
- Views to the bay
- Views to the hills
- Trecco Bay Caravan Park

Density and Heights

Generally a lower density is proposed in Sandy Bay compared with other areas of the development.

Built Form and Massing

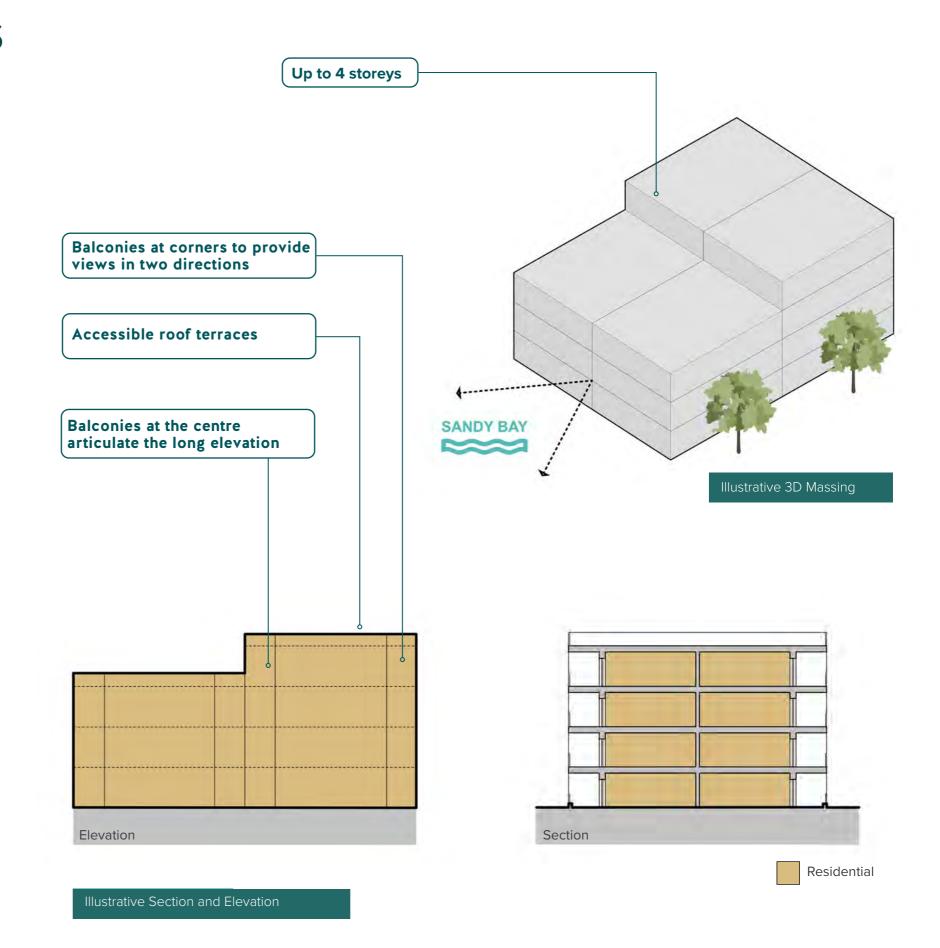
The apartment's built form affords more residents views of the sea. The massing is simple and restrained and subtly animated with recessed and projecting balconies. Shared amenity roof space is introduced to benefit from long range waterfront views.

Block Typology

A rectilinear apartment block is proposed. There is an ambition to include partial basement parking beneath the block, reducing visual impact of surface-level parking.

Dwelling and Unit Typology

Typologies predominantly consist of apartments.

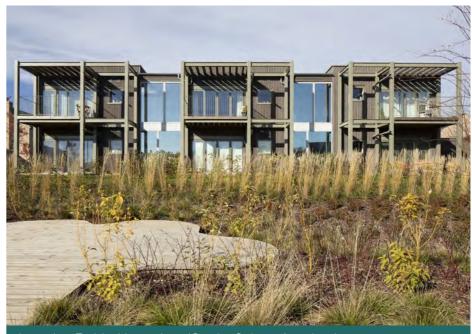




Projecting balconies provide better views of the sea and articulate the façade

Accessible roof terraces provide shared semi-private amenity space for residents

Sheltered recessed balconies



Lovedon Fields, Hampshire (Credit: Galerie Ansehen)

Boundaries

Shallow planted defensible spaces are proposed for ground floor residential units.

Private Amenity

Private amenity space is provided through recessed and projecting balconies. In addition, semi-private rooftop terraces for residents maximises views to the sea to the south and hills to the north.

Detailing and Materials

The elevations consist primarily of slate or a grey-coloured brick. Timber is introduced into the sheltered balcony areas. Decorative metalwork is proposed for the balconies and solar shading devices.



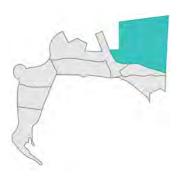


Accordia, Cambridge

Precedents illustrating new residential developments

4.19 SANDY BAY VILLAGE

Character Area Overview



Character Generators

- Active travel links
- Views to the bay
- Views to the hills
- Newton Primary School
- Trecco Bay Caravan Park

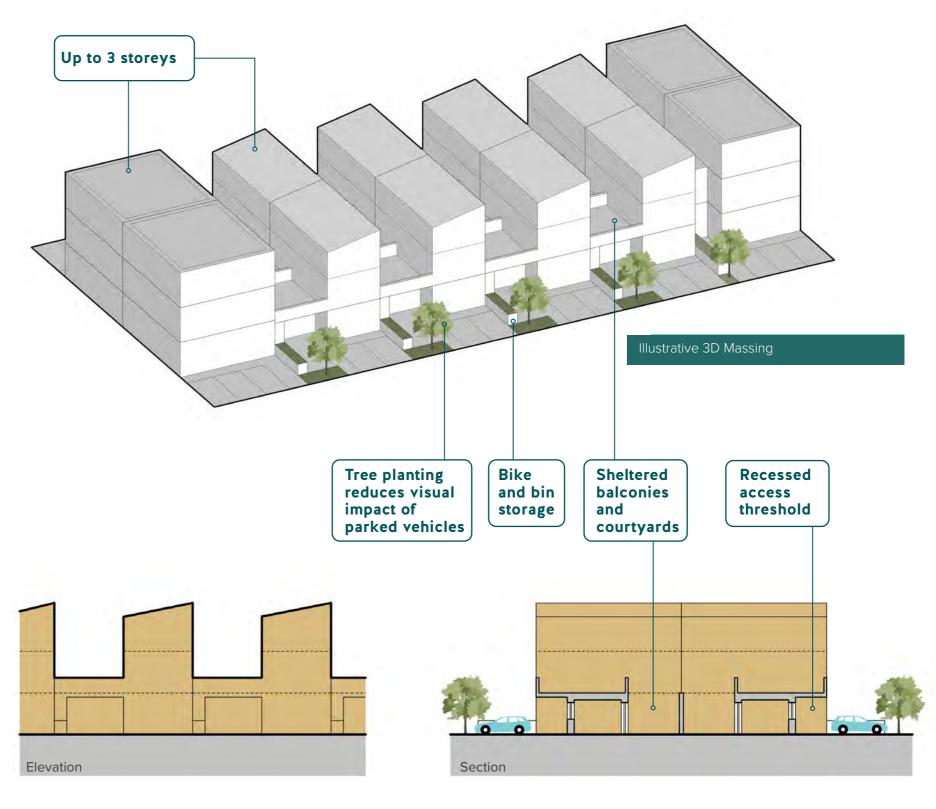
Density and Heights

Generally a lower density is proposed in Sandy Bay compared with other areas of the development and massing reflects a more intimate scale.

Built Form and Massing

Some properties consists of back-to-back dwellings with balconies and courtyards. Private amenity space is sufficient, but greater emphasis is placed on shared public amenity space neighbouring this terrace typology. The terrace run consists of a prominent typology, however there is the opportunity to 'book-end' the dwellings by different, more larger dwellings.

There is an ambition for some units to be 3 storeys, with a ground floor courtyard and first floor terrace. The entrance can be recessed, creating a covered, sheltered access threshold. There is an opportunity to introduce a roof pitch, angled at 35 degrees, providing the optimum angle for photovoltaics. A low-level timber storage unit to the front of the property provides a concealed space to store recycling bins and bicycles.



Illustrative Section and Elevation

Illustrative 3D Massing

Another unit type consists of more traditional dwellings with back-to-back rear gardens. The main terrace run consists of two dwelling types, arrayed and mirrored to create subtle variety across the terrace length.

There is an ambition for some dwellings to be 2/3 storeys, with a rooftop terrace at the second floor. The roof to the 3 storey element is proposed to be flat or a shallow pitch to reduce the overall perceived height.

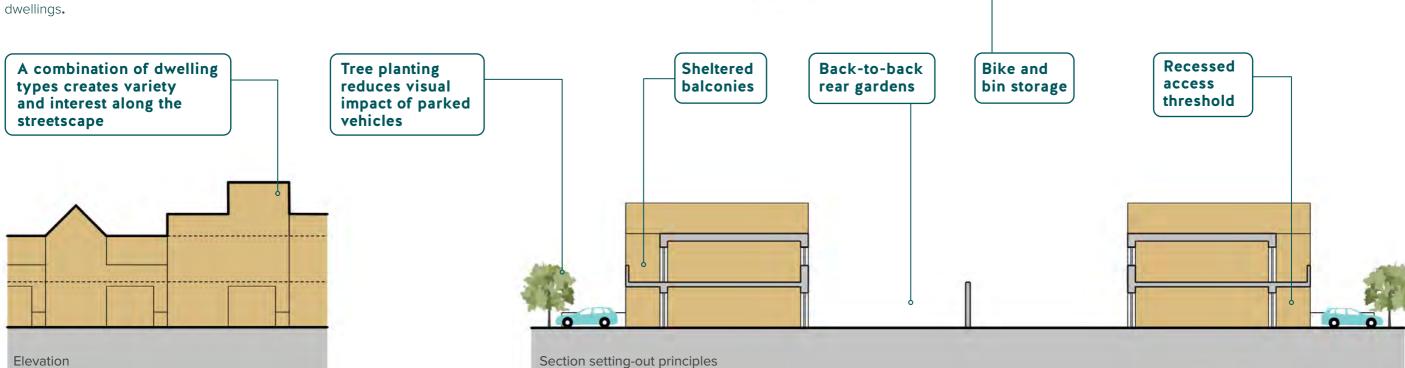
There is an ambition for unit types to be limited to 2 storeys, but with a gable fronting the street to create a more varied and articulated roof line. A recessed covered entrance threshold is consistent in both. A low-level timber storage unit at the front of the property is provided storage for bins and bicycles.

Block Typology

Residential terrace typology is proposed

Dwelling and Unit Typology

Typologies predominantly consist of terraced, detached and semi-detached dwellings.



2/3 storeys

2 storeys

Illustrative Section and Elevation

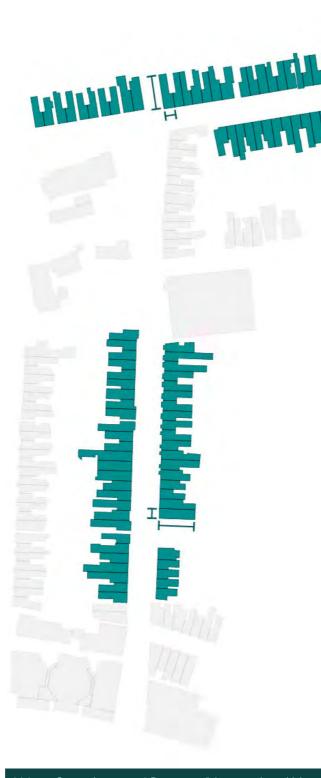
Residential

Porthcawl Waterfront Regeneration

Contextually Responsive Structure

The proposed layout for Sandy Bay Village has been shaped by a close reading of Porthcawl's historic urban grain. Traditional streets within the town centre are typically arranged perpendicular to the coastline, examples such as Picton Avenue, Esplanade Avenue and Mary Street demonstrate this distinctive pattern. These streets form long, elegant vistas defined by a rhythmic sequence of townhouses and active frontages, contributing to Porthcawl's unique seaside character.

The masterplan for Sandy Bay Village draws directly from this precedent. By reinterpreting the perpendicular street structure, the layout establishes a familiar and contextually appropriate pattern that feels "of Porthcawl." In doing so, the scheme avoids the disconnected cul-de-sac arrangements often found in contemporary suburban developments. Instead, it promotes a permeable and legible neighbourhood that is easy to navigate, encourages walking, and reinforces a strong sense of place.



Urban Grain Appraisal Diagram (Victoria Ave / Mary St)

Democratic Views and Appreciation of Landscape

A key objective of the design has been to ensure that the natural landscape is a shared asset rather than a privileged one. The perpendicular street alignment, combined with the introduction of projecting bays, balconies, and carefully positioned gable ends, creates frequent and meaningful views towards the dunes and the rolling hills to the north.

This approach supports a "democratic appreciation" of the setting, allowing the majority of homes to enjoy outward-facing connections to the wider environment. Homes are not isolated from their context; rather, they are visually anchored within it, ensuring that the landscape contributes to everyday life throughout the neighbourhood.

Established and Successful Urban Precedent

The principles underpinning the layout are reinforced by well-established national precedents. The Accordia development in Cambridge is a particularly relevant comparator: its grid-based street structure achieves a refined balance between density, character and liveability. Residents of Accordia consistently report a strong sense of community, much of which arises from the interplay between the built form, shared courtyards, and informal communal spaces.

'Sandy Bay Village' builds on these proven ideas, adapting them to a coastal context, to create a neighbourhood that is both efficient in its land use and rich in social opportunity. The structure supports natural surveillance, encourages neighbourly interaction, and underpins a comfortable, human-scale environment.



Accordia Masterplan (Credit: Feilden Clegg Bradley Studios)





Accordia Photos (Credit: Feilden Clegg Bradley Studios)

Efficient and Sustainable Urban Form

The proposed grid layout offers clear spatial and environmental benefits. By organising homes within compact plots and reducing the extent of road infrastructure, it achieves higher densities without compromising comfort or privacy. The resulting efficiency ensures that a greater proportion of land can be dedicated to high-quality public realm, including tree-lined streets, pocket spaces, and areas for play and social activity.

This approach enhances the walkability of the neighbourhood and reduces car dominance, creating quieter, greener, and more attractive streets. The efficient street structure also simplifies servicing and emergency access, supporting long-term maintenance and operational resilience.

Variety Within a Coherent Framework

While the overall structure is intentionally clear and ordered, variety is carefully introduced to ensure architectural richness and a distinctive sense of identity. Façades vary in texture, tone, and materiality, while subtle differences in building typologies and courtyard arrangements add depth and character to the streetscape.

This balance between order and diversity helps avoid monotony, allowing the neighbourhood to feel cohesive yet varied. The resulting environment is visually engaging, rooted in local character, and capable of evolving organically over time.



Complementary colours at first floor

Timber to entrance thresholds and balconies

Masonry ground floor creating consistency across dwelling types

Boundaries

For most dwellings, a modest defensible space defines the front boundary, consisting of parking, bin and bike storage and areas for trees and soft landscape. A masonry wall is used to define courtyard boundaries to the rear in some locations. In other locations, the rear boundaries are defined by timber / masonry walls around gardens.

Private Amenity

Private amenity space is incorporated through small front gardens, larger back gardens and upper terraces and balconies.

Detailing and Materials

For all dwellings a grey brick masonry plinth is proposed. This is interrupted by timber at entrances to properties. At first floor and above, there is an ambition for different materials to be used, creating variety and texture to the street. Slate is proposed along with pastel-coloured timber and render. Grey standing seam metal forms the roofs above, creating material continuity with other character areas.





4.20 HOMES & TYPOLOGIES

Introduction

The layout of homes on the masterplan illustrates a potential distribution of a range of typologies across the site which will provide the required number of homes in alignment with the density ranges.

Compliance with Standards

Proposed home are in accordance with the following standards.

- The Welsh Housing Quality Standard 2023 (April 2024)
- Lifetime Homes Design Guide October 2011 & Lifetime Homes Standards Checklist April 2015
- Technical housing standards nationally described space standards (NDSS) March 2015 (Amended 19 May 2016)
- London Housing Design Guide 2010

Accessibility

Homes will be accessible and easily adaptable. As a minimum requirement, the dwellings meet the accessibility standards within Approved Document M(4) Category 2. In addition. There is an opportunity to provide some homes which meet or are capable of meeting 'M(4) Category 3 – Wheelchair user dwellings'.

Space and Comfort

All homes have access to private or shared private external amenity space. This can be provided through balconies, terraces, courtyards, front and rear gardens. Boundary treatments provide a reasonable degree of privacy from neighbouring homes and streets. Care has be taken at ground floor thresholds between public and private space.

Natural Light and Ventilation

Homes are designed to utilise natural light and ventilation. Dual aspect dwellings have been encouraged where possible and floor depths limited to allow key habitable spaces to be cross-ventilated or utilise single-sided ventilation. Single aspect north-facing dwellings have be avoided where possible.

Sustainability

It is an ambition for an energy hierarchy strategy to be followed for all buildings, prioritising reduced operational and embodied energy before considering renewables, such as photovoltaic (PV) panels. A 'fabric first' approach can be adopted in the first instance, ensuring an efficient form factor, sensible window placement and orientation and a robust thermal envelope.

Passive design principles are followed, such as using deciduous planting or solar shading to reduce overheating, natural ventilation, rainwater harvesting and grey water systems. Sustainable urban drainage systems (SuDS) have been encouraged within the private domain where possible.

Energy Generation

It is an ambition for homes to be capable of accommodating air source heat pumps or similar. The mechanical equipment should not be visible from the street and any noise mitigation measures implemented. Roofs are capable of accommodating photovoltaic (PV) panels, ideally seamlessly integrated into the roof finish to avoid visual clutter. Homes are capable of accommodating electric vehicle charging points, ideally located within the storage unit to the front of the properties.



Sandy Bay Village homes

4.21 **BUILT FORM: MATERIALS PALETTE**

Materials

The development reflects the varied use of materials throughout the existing town and pays homage to recognised heritage structures and their settings. Proposed materials are robust and well detailed and able withstand the coastal environment, aging well and developing a rich patina over time.

Dominant Materials

Masonry is the dominant material throughout all character areas. Rough dressed natural grey stone would complement many of the existing robust harbour buildings and sit comfortably alongside recent contemporary additions, such as the new masonry building in cosy corner. In addition to natural stone, multi grey bricks are also proposed. The subtle variation of tone ads texture and interest to the masonry elements and weathers sympathetically.

Projecting / recessed brick headers and variation in brick bonds adds subtle sophisticated variety within the consistency of the material.

Timber is proposed throughout but used without treatment, allowing the material to weather to a silver-grey. Careful timber detailing is implemented to ensure longevity, such as lifting it off the ground through the introduction of a masonry plinth and ensuring end grains are protected to reduce water ingress.

Accent Materials

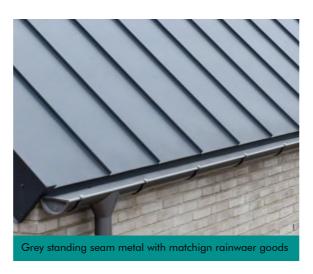
The calmness on consistency of the grey masonry and timber is occasionally interrupted by colour. Taking inspiration from the delightful Victorian pastelcoloured façades along Mary Street, a mixture of coloured renders are proposed to help provide variation across the development. A through colour render, rather than an applied paint finish would be recommended for greater longevity. Careful detailing such as projecting sills and a projecting parapet capping will ensure water is better managed and minimise staining.

The use of coloured standing seam could also be used to introduce colour and vertical or horizontal grain and texture. Anodizing would be recommended over power-coating to ensure greater durability and longevity

Roofs are kept simple and with a minimal colour palette, allowing the render and metal to add coloured variation across an elevation. The use of grey standing seam and dark grey tiles should be used. Welsh slate for the roofs and the walls is encouraged. Fine metalwork for the balusters and handrails are encouraged, reflecting the fine ironwork within many of the Victorian terraces. Rainwater goods should be robust. It is an ambition to use galvanised steel or dark grey metal.







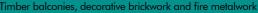














05 | ACCESS AND TECHNICAL STRATEGIES

5.1 ACCESS

Vehicular Access

The majority of development in Sandy Bay and Coney Beach will have access off a new small three arm roundabout on Eastern Promenade. This will also provide direct links to The Portway roundabout facilitating onward connections.

Residential and commercial areas within the Salt Lake area will be accessed via existing junctions along The Portway. Vehicular movement along the Promenade will be limited to emergency and servicing vehicles during designated times.

The existing Mackworth Road will be reconfigured to terminate at the proposed Linear Park, as a result it will no longer provide vehicle access into the site. An appropriate turning head will be provided to allow for manoeuvring.

Pedestrian/Cycle Access

It should be noted that all vehicular access points will also facilitate pedestrian/cycle connections.

With regard to facilitating active travel, existing access points around the periphery of the site will be improved to help integrate the development with the wider context. This will be particularly important to the town centre and Trecco Bay.



Key Junction

The proposed Spine Road serving Coney Beach and Sandy Bay will necessitate the formation of a new junction on Eastern Promenade to accommodate anticipated traffic flows and provide efficient access to both of the aforementioned development areas. Full technical details will be submitted as part of a detailed planning application; however, in summary, the junction is proposed as a compact three-arm roundabout.

The existing southern spur connecting to Eastern Promenade will be removed to simplify the layout, reduce potential conflict points, and improve overall safety and flexibility of the public realm within Salt Lake.

The design will incorporate appropriate active travel connections and pedestrian crossing points to ensure safe, direct, and comfortable movement for all users. Consideration will also be given to the relationship between the junction and adjacent public realm areas to maintain a coherent and well-integrated streetscape.



5.2 COASTAL DEFENCES

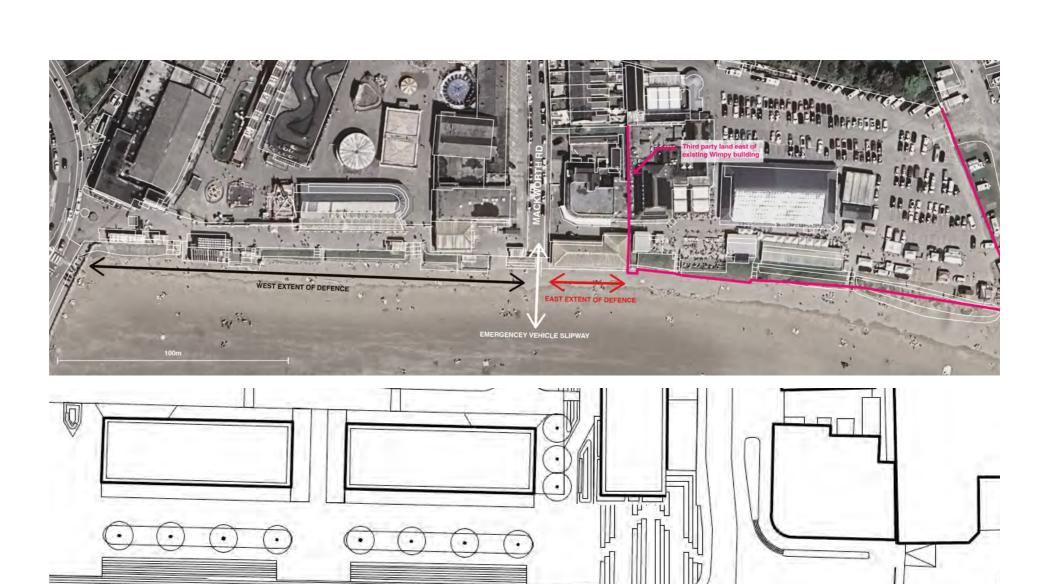
Design Concept

ARUP's latest coastal defence design has been reviewed and adopted as the basis for future proposals. The overarching approach seeks to combine flood protection with opportunities to enhance placemaking and public amenity along the shoreline.

The proposals include the integration of build-outs within the stepped revetment wall, which will function as viewpoints and provide seating areas away from the main pedestrian route along the Boardwalk. The inclusion of land to the east of Mackworth Road as part of the concept allows for the lowering of Mackworth Road, reducing the extent of the radius slipway onto the beach.

A multi-functional, amphitheatre-style amenity space is incorporated into the design by extending the slipway crest level inland and adjusting the corresponding slipway and road levels. This approach creates usable public space while maintaining the primary function of coastal defence.

Overall, the concept delivers a strategically important piece of flood defence infrastructure, reinforcing the interface with Sandy Bay Beach and supporting a key east west coastal connection, while also enhancing the public realm and recreational opportunities along the coastline.



Flood Defence Concept Layout (





5.3 Sustainable Drainage

Sustainable Urban Drainage System (SuDS) Strategy

The proposed sustainable drainage strategy has been developed in accordance with SAB requirements to provide a resilient, multifunctional approach to surface water management within the Porthcawl masterplan. The strategy integrates a range of measures designed to manage runoff sustainably while enhancing the visual quality, biodiversity, and amenity of public spaces.

Attenuation features, including basins and swales, are proposed to control discharge rates and provide temporary storage during peak rainfall events, reducing the risk of downstream flooding. These features are designed to be multifunctional, providing amenity value, supporting habitat creation, and enhancing the public realm. Swales are incorporated to convey runoff, naturally filter pollutants, slow water movement, and create green corridors that support both biodiversity and passive recreation.

Permeable paving is proposed across hardstanding areas to promote infiltration and reduce surface runoff. SuDS tree pits are integrated throughout the public realm to capture and filter rainfall at source, providing both drainage and urban greening benefits, including shade and habitat for wildlife. Rain gardens are strategically positioned within landscaped areas to further improve water quality, support biodiversity, and offer attractive amenity and recreational space for residents and visitors.

Together, these measures form a comprehensive, SAB-compliant SuDS framework that addresses flood risk management, water quality, and urban greening in a holistic manner. The approach ensures surface water is treated as a design asset, creating multifunctional spaces that enhance resilience, ecological value, and the overall quality of the public realm.



5.4 ENERGY & SUSTAINABILITY

The regeneration of Porthcawl Waterfront has been guided by the ambition to create a sustainable, resilient, and forward-looking coastal community. The masterplan responds directly to national and local policy objectives, including Planning Policy Wales, the Well-being of Future Generations (Wales) Act, and the Bridgend Replacement Local Development Plan (2018–2033), embedding sustainability principles into every stage of design and delivery.

Sustainable Design Approach

In terms of facilitating active travel, existing access points around the periphery will be improved to integrate the development with the wider context. This will be particularly important to the town centre and Trecco Bay.

The development aims to establish a framework for energy-efficient and low-carbon living through a combination of passive design, efficient building fabric, renewable technologies, and a landscape-led approach that enhances environmental performance.

Fabric-first energy efficiency can be achieved through prioritising high levels of insulation, airtightness, and optimised solar orientation where possible to reduce energy demand before reliance on technology.

Renewable and low-carbon technologies are integrated through potential air source heat pumps, photovoltaic panels, and future-ready energy infrastructure to support a transition to net-zero carbon.

The principles of the regeneration of the water front encourage the use of responsibly sourced, low-embodied carbon, and locally available materials that are durable in the coastal environment. The proposals also advocate modular and adaptable construction techniques that can reduce waste and enable future re-use of materials.

Climate Resilience and Water Management

Building upon the Sustainable Drainage (SuDS) strategy, the masterplan integrates natural systems and landscape design to manage rainfall, mitigate flood risk, and strengthen ecological resilience:

Integrated green and blue infrastructure, including rain gardens, swales, and permeable paving, provides visual amenity and biodiversity value while managing runoff at source.

Flood resilience measures ensure development remains safeguarded from coastal and surface water flooding, in accordance with the latest Welsh Government guidance.

Drought and heat resilience are addressed through the specification of native and drought-tolerant planting, together with passive cooling and shading strategies in building and public realm design.

Sustainable Movement and Lifestyle

The proposals promote a sustainable lifestyle that reduces car dependency and encourages active, low-carbon travel through:

- Active travel networks that link the development seamlessly to the town centre, primary school, seafront, and wider coastal path, encouraging walking and cycling for everyday journeys.
- Electric vehicle (EV) charging infrastructure is proposed throughout the site to support the ongoing transition to cleaner transport modes.
- Public transport integration will connect new neighbourhoods to existing bus routes, ensuring sustainable accessibility for residents and visitors alike.

Biodiversity and Environmental Enhancement

The landscape strategy contributes to wider ecological networks and coastal biodiversity through:

- Retention, enhancement and expansion of the sand dune and maritime grassland habitats as part of the site's green infrastructure.
- Creation of new ecological corridors linking Griffin Park to the restored dunes, supporting wildlife movement and habitat continuity.
- Planting design that reflects local coastal character, enhances carbon sequestration, and strengthens resilience to climate change impacts.

Community Well-being and Long-Term Stewardship

The sustainability strategy is not limited to environmental performance; it also seeks to enhance quality of life and community wellbeing. This includes:

- Provision of high-quality public spaces that encourage social interaction, recreation, and outdoor activity.
- Opportunities for community engagement in sustainability initiatives such as community gardens and biodiversity stewardship.
- A long-term management and maintenance framework to ensure that environmental assets are preserved and enhanced over time.

Housing Standards

The proposed residential development has been designed to meet current Welsh energy efficiency and sustainability standards, reflecting a commitment to reducing carbon emissions and supporting low-carbon living. Homes are designed to achieve high levels of thermal performance, incorporating well-insulated building envelopes, energy-efficient windows, and low-energy heating systems to minimise operational energy use.

The design approach aligns with the Welsh Government's Future Homes Standard and relevant Building Regulations, ensuring that dwellings contribute to national climate and energy targets. Low-carbon technologies, such as air-source heat pumps, photovoltaic panels, and energy-efficient lighting, are considered where feasible to reduce reliance on fossil fuels and support on-site renewable energy generation.

Summary

Together, these measures will ensure the Porthcawl Waterfront regeneration delivers a resilient, low-carbon, and inclusive coastal community. The proposals seek to meet and, where possible, exceed current Welsh sustainability standards, supporting the wider ambition for a greener, healthier, and more prosperous Wales as envisioned in the Well-being of Future Generations Act.

5.5 SECURE BY DESIGN

Passive Surveillance, Safety and Community Cohesion

A key aspect of the Porthcawl Waterfront masterplan is the arrangement of buildings and public spaces to promote passive supervision across the public realm. This approach has been central to shaping a layout that feels safe, welcoming, and well-connected, particularly where development fronts onto open coastal landscapes and key pedestrian routes such as the Wales Coastal Path.

Rather than adopting a purely efficient or inwardfacing arrangement, the masterplan positions homes and active frontages to overlook the proposed network of public spaces, green corridors, and pedestrian routes previously set out within the DAS. This ensures that natural surveillance is maximised and that the interface between built form and open space contributes to a sense of comfort and belonging for the site.

The masterplan promotes positive edges to key public areas, ensuring that open spaces such as the coastal park, nature corridors, and meanwhile activity zones are well observed and integrated into daily life. This approach allows residents and visitors to experience the natural landscape as part of the town's living fabric, enhancing the overall quality and safety of the public realm.

To support this, a site-wide lighting strategy has been developed to create a safe and legible environment during the evening, while being sensitively designed to minimise glare and disruption to the local coastal ecology and dark skies.

The masterplan also seeks to establish a cohesive and inclusive community, with a strong sense of place and identity. The inclusion of a well-overlooked community parks, together with mixed-use and leisure facilities along the waterfront, will help encourage activity throughout the day and into the evening.

Legibility is central to the urban structure, with primary pedestrian routes and cycle connections following natural desire lines that link the coastal path, the marina, and the town centre. This hierarchy of routes creates a recognisable network of streets and spaces that support easy navigation and reinforce visual connections to the sea. Cul de sacs have been avoided to promote permeability and avoid isolated areas, ensuring a coherent and connected urban grain.

Where boundaries are required, they have been designed to reflect a natural and open character, using planting, low walls, and sustainable drainage features to define space subtly without creating visual barriers. Swales and native planting will help delineate thresholds between public and private areas while enhancing biodiversity and supporting surface water management.





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